

STAFF REPORT
PLANNING COMMISSION

FILE NO.: PDC09-023

Submitted: 1/25/10

PROJECT DESCRIPTION: Planned Development Rezoning from LI Light Industrial to A(PD) Planned Development Zoning District to allow construction of a food mart / car wash building and other upgrades to an existing gas station on a 0.6 gross acre site.

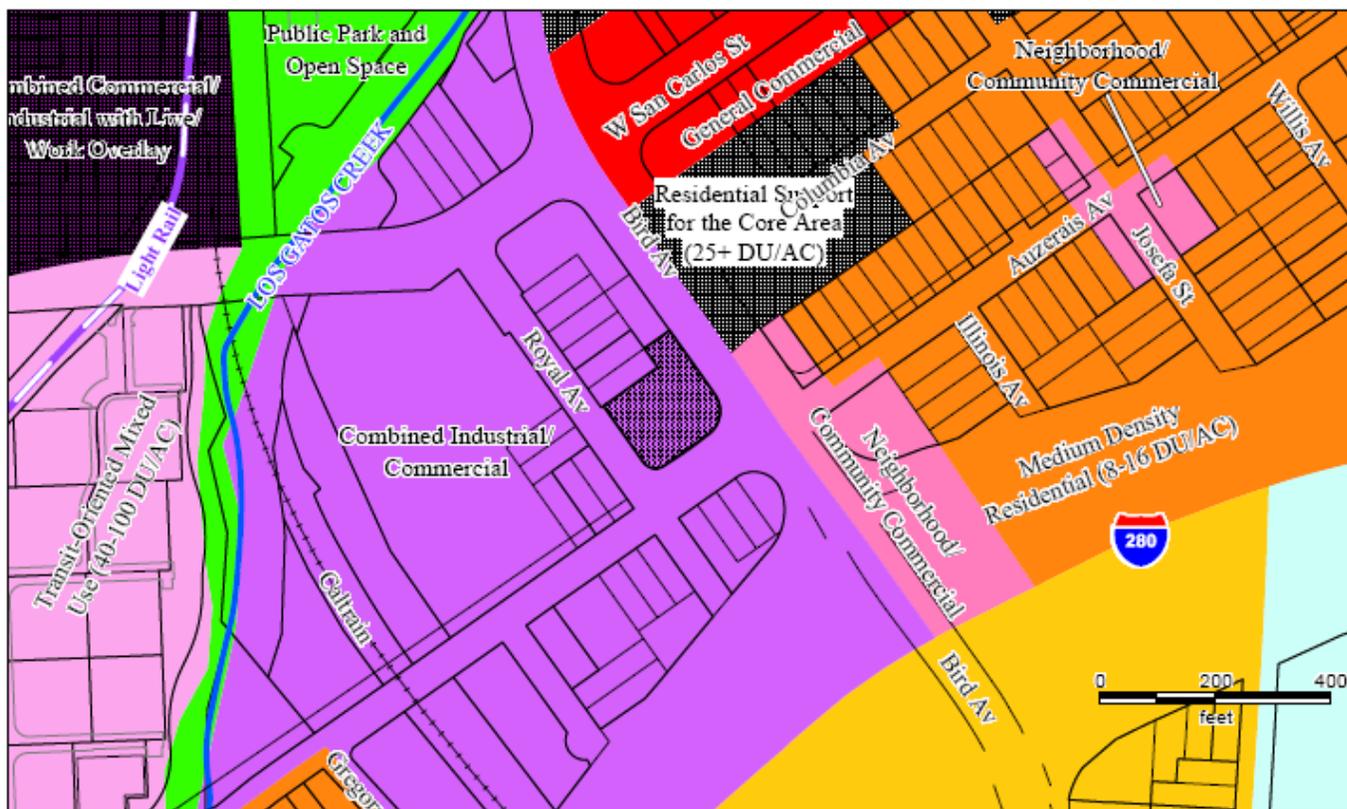
Existing Zoning	LI Light Industrial
Proposed Zoning	A(PD) Planned Development
General Plan	Combined Industrial Commercial
Council District	6
Annexation Date	3/16/1911
SNI	Burbank / Del Monte
Historic Resource	No
Redevelopment Area	SNI
Specific Plan	N/A

LOCATION: North side of Auzerais Avenue, between Bird Avenue and Royal Avenue.

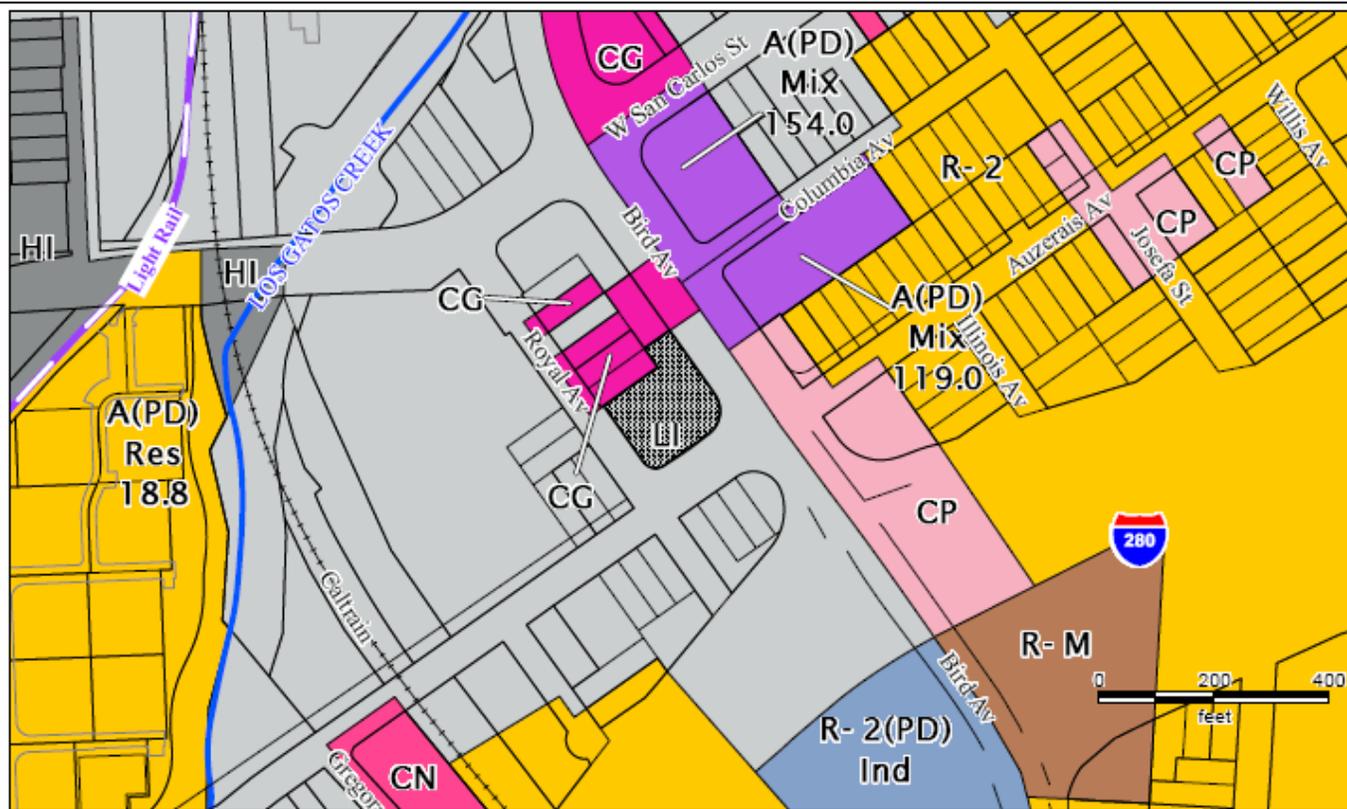
Aerial Map



GENERAL PLAN



ZONING



RECOMMENDATION

Planning staff recommends the Planning Commission forward a recommendation to the City Council to approve the proposed Planned Development Rezoning for the following reasons:

1. The proposed project of a food mart, car wash and gas station is consistent with the San José 2020 General Plan Land Use/Transportation diagram designation of Combined Industrial Commercial in that this designation is intended for commercial uses.
2. The redeveloped of this site is also consistent with the General Plan Commercial Land Use Policy # 4 that encourages the upgrade, beautification, and revitalization of existing commercial areas.
3. The proposed zoning is compatible with existing uses on the adjacent and neighboring properties.
4. The proposed project is in substantial conformance to the Commercial Design Guidelines.

BACKGROUND

On June 3, 2009, the applicant, Muthana Ibrahim, filed a Planned Development Rezoning from LI Light Industrial to A(PD) Planned Development Zoning District to allow demolition of an existing auto service building and two gas pumps, and to construct a food mart and car wash building and other upgrades to the existing gas station on a 0.6 gross acre site, located on the north side of Auzerais Avenue, between Bird Avenue and Royal Avenue. In February 2006, a zoning ordinance change eliminated the restriction on combining service stations and retail sales of food and beverages, paving the way for service station remodels such as this one.

A Planned Development Rezoning is required because the developer is proposing to develop the property in a configuration that is not supported in any of the City's conventional zoning districts. Specifically, the project proposes a food mart and car wash building with a minimal setback from the front property lines. Surrounding uses include commercial businesses to the north, commercial / industrial businesses to the west and south, and multi-family uses to the east.

PROJECT DESCRIPTION

The existing 2,292 square foot auto service building and two gasoline pumps will be demolished to make way for the construction of a 3,816 square foot food mart/car wash building which includes a 550 square foot office on the second floor. Remaining on the site will be a canopy, four gasoline pumps and underground fuel storage tanks. The parking area will be slightly rearranged to accommodate 13 parking spaces on site. The site will take access only from Bird Avenue and Auzerais Avenue.

The project site is located in an alcohol sales restrict area. The project does not include off-sale of alcohol at this time. If at a later date the applicant wishes to sell alcohol, because the proposed zoning includes off-sale of alcohol as a conditional use, the approval of a Planned Development Permit and a Determination of Public Convenience or Necessity would be necessary.

ANALYSIS

The proposed Planned Development Rezoning would facilitate redevelopment of an existing gasoline station. This project has been evaluated for consistency with the Commercial Design Guidelines and Drive-Through Policy with respect to the following key areas: 1) General Plan Conformance, 2) Strong Neighborhood Initiative, 3) Zoning and Design Guideline Conformance, and 4) Environmental Review.

General Plan Conformance

This site has a designation of Combined Industrial Commercial on the adopted San José 2020 General Plan Land Use/Transportation Diagram. The proposed project of a food mart and car wash is consistent with the San José 2020 General Plan Land Use/Transportation diagram designation of Combined Industrial Commercial in that this designation is intended for commercial uses. The Combined Industrial Commercial land use designation is intended for commercial, office, or industrial developments or a compatible mixture of these uses. The uses of the Industrial Park, Light Industrial, General Commercial and Neighborhood/Community Commercial land use categories are consistent with this use category. Uses should be arranged on the site in a manner that avoids land use incompatibilities. Commercial Land Use Policy # 4 encourages the upgrading, beautifying, and revitalization of existing strip commercial areas and shopping centers. As this project is proposing to revamp and upgrade an existing gas station it meets the intent of the Commercial Land Use Policy # 4.

Strong Neighborhood Initiative (SNI)

The subject site is located within the Burbank/Del Monte Strong Neighborhood Initiative Area. Item # 2 of the Burbank/Del Monte SNI Action Plan is to “Designate Scott Street and Auzerais Avenue as the primary pedestrian/bikeway through the neighborhood and complete streetscape improvements”. As a part of this project, the applicant will be required close unused driveways and replace any curb, gutter, sidewalk damaged during construction. Handicap ramps will be upgraded and a 4 foot median island will be constructed along Auzerais Avenue to prevent left turns from the site.

Zoning and Design Guideline Conformance

Project Design / Neighborhood Compatibility

As proposed, the new food mart and car wash building will be built closer to Royal Avenue, however, it would face onto Bird Avenue replicating the existing configuration of the site. Planning staff would normally encourage new buildings to be placed as close to the main intersection (Bird/Auzerais) as possible, however, given the existing canopy, gasoline pumps and underground storage tanks, placement of the building towards the rear of the property was the best solution for this site. The new food mart/car wash building will have a parapet style that will work well with the new modern buildings across Bird Avenue.

Perimeter Setbacks

The Commercial Design Guidelines recommend a 25 foot street setback for service station and car wash uses. However, the Guidelines also encourage a five (5) foot minimum and ten (10) foot average street setback for areas having the characteristics of a sidewalk strip street. As the project is within an area of the Burbank/Del Monte SNI plan that encourages walking and biking, staff is recommending a

six (6) foot setback on all three street frontages and a zero (0) setback for the interior/side setback to create a more interesting and walkable environment for residents.

Parking

For a project with a variety of use on the site, the Zoning Ordinance calls for one (1) parking space per employee, one (1) parking space for the air and water pumps, one (1) parking spaces for informational stops, one (1) parking space for every 200 square feet of retail, one (1) parking space for every 250 square feet of office space, and five (5) stacking spaces for a self-service car wash. The project proposes thirteen (13) parking spaces, and stacking space for five (5) cars in the driveway / queuing area before the car wash.

As proposed, the food mart would have a parking requirement of 8.8 spaces, the office would require 1.9 spaces, the site will also need to provide an informational parking space, and an air/water space for a total of 13 parking spaces. As the food mart parking requirement includes space for employees and there are no separate employees for the gas station or car wash uses, the proposed parking complies with the City's parking requirements.

Environmental Review

Under the provisions of Section 15302 of the State Guidelines for Implementation of the California Environmental Quality Act (CEQA), this project is found to be exempt from the environmental review requirements of Title 21 of the San José Municipal Code, implementing the California Environmental Quality Act of 1970, as amended. The proposed building is a replacement and will have substantially the same purpose as the existing structure. Staff has review the noise levels from the proposed car wash and determined that it will not increase the DNL of the site given the existing elevated ambient noise level for an area near a freeway. In addition, the applicant's conformance to City Council Policy 6-29 and the C.3 requirements for stormwater runoff make impacts to water quality less than significant as well. These items will be addressed in more detail through the Planned Development permit process.

Conclusion

Planning staff recommends approval of the proposed Planned Development Rezoning as the applicant has worked diligently with City staff to create a project that utilizes the existing infrastructure on the site while still providing a positive addition to the neighborhood along Bird Avenue.

PUBLIC OUTREACH

The project is located in the Burbank / Del Monte SNI area. A community meeting was not held since this project represents only a modest change to the original use and conforms to the Commercial Design Guidelines. A sign was posted on-site to notify neighbors of the proposed development. The rezoning was also published in a local newspaper, the Post Record. This staff report is also posted on the City's Website. Staff has been available to respond to questions from the public. A notice of this Planning Commission public hearing and subsequent City Council hearing was mailed to the owners

and tenants of all properties located within 500 feet of the project site and posted on the City website. This staff report is also posted on the City's Website. Staff has been available to respond to questions from the public.

Project Manager: Jodie Clark, AICP **Approved by:** Jean Hammett **Date:** 2/2/10

Owner/Applicant:	Attachments:
Vintners Distributors, Inc. Sunny Goyal 41805 Albrae Street, 2 nd floor Fremont, CA 94538 MI Architacts, Inc. Muthana Ibrahim 2960 Camino Diablo, Suite 100 Walnut Creek, CA 94597	Development Standards Exemption Final Public Works Memo Reduced Plan Set

GENERAL DEVELOPMENT PLAN NOTES PDC09-023

PDC09-023

The following notes are to be incorporated on the Final General Development Plan upon City Council approval. These notes reflect the modifications recommended by the Planning Commission and shall replace all other notes currently identified on said plans.

Development Standards

Allowed Uses: All permitted uses as allowed in the CIC – Combined Industrial / Commercial Zoning District per Title 20 of the San Jose Municipal Code, as amended. Conditional Use and Special Uses of the CIC – Combined Industrial / Commercial Zoning District may be considered through the Planned Development Permit process, as stated in Chapter 20.100 of the Zoning Code.

Parking Requirements: All land uses shall provide on-site parking in conformance with Chapter 20.90 (Parking and Loading) of the City of San Jose, Municipal Code, as amended.

Building and Parking Setbacks: All three street frontages– Minimum six feet (6’)
Side / Rear – Zero feet (0’)

Maximum Building Height: The maximum building height shall not exceed forty-five feet (45’), and no more than two (2) stories will be permitted.

Parking: Parking shall be provided in conformance with the City’s Zoning Ordinance

Design Guidelines: The proposed development shall conform to the Commercial Design Guidelines, unless otherwise indicated.

Drive-Through Uses: No more than one Drive Through operation may exist on the site at any one time, in conformance with the City’s Drive-Through Policy. Any change in use from one enumerated use to another as set out in Title 20 shall be evaluated through a Planned Development Permit/Amendment to the satisfaction of the Director of Planning.

Signs: All signs shall conform to current City of San Jose sign regulations (Title 23) and shall be approved under a separate Planned Development Adjustment permit to the satisfaction of the Director of Planning.

Lighting: All lighting or illumination shall conform to any lighting policy adopted by City Council.

Late Night Use and Activity: No establishment on the site shall be open between the hours of 12:00Midnight and 6:00 am except pursuant to and in compliance with a Planned Development Permit/Amendment.

GENERAL DEVELOPMENT PLAN NOTES PDC09-023

Noise:

Post-construction mechanical equipment shall conform to the City’s General Plan limitation of 60DNL at the property line.

Water Quality

Prior to the issuance of a Planned Development Permit, the applicant must provide details of specific Best Management Practices (BMPs), including, but not limited to, bioswales, disconnected downspouts, landscaping to reduce impervious surface area, and inlets stenciled “No Dumping – Flows to Bay” to the satisfaction of the Director of Planning, Building and Code Enforcement.

The project shall comply with Provision C.3 of NPDES permit Number CAS0299718, which provides enhanced performance standards for the management of stormwater of new development.

The project shall comply with applicable provisions of the following City Policies – 1) Post-Construction Urban Runoff Management Policy (6-29) which establishes guidelines and minimum BMPs for all projects and 2) Post-Construction Hydromodification Management Policy (8-14) which provides for numerically sized (or hydraulically sized) TCMs.

Tree Removals.

Unless otherwise approved on the Site Plan, no tree larger than 56 inches in circumference, at a height of 24 inches above the natural grade slope, shall be removed without a Planned Development Permit Amendment as issued by the Director of Planning.

Tree Mitigation. Removal of any trees shall be mitigated at the following ratios:

Diameter of Tree to be Removed	Type of Tree to be Removed			Minimum Size of Each Replacement Tree
	Native	Non-Native	Orchard	
18 inches or greater	5:1	4:1	3:1	24-inch box
12 - 18 inches	3:1	2:1	none	24-inch box
less than 12 inches	1:1	1:1	none	15-gallon container

x:x = tree replacement to tree loss ratio

Note: Trees greater than 18” diameter shall not be removed unless a Tree Removal Permit, or equivalent, has been approved for the removal of such trees.

Archaeology.

Pursuant to Section 7050.5 of the Health and Safety Code, and Section 5097.94 of the Public Resources Code of the State of California in the event of the discovery of human remains during construction, there shall be no further excavation or disturbance of the site or any nearby area reasonably suspected to overlie adjacent remains. The Santa Clara County Coroner shall be notified and shall make a determination as to whether the remains are Native American. If the Coroner determines that the remains are not subject to his authority, he shall notify the Native American Heritage Commission who shall attempt to identify descendants of the deceased Native American. If no satisfactory agreement can be reached as to the disposition of the remains pursuant to this State law, then the land owner shall re-inter the human remains and items associated with Native American burials on the property in a location not subject to further subsurface disturbance.

In any cases where the graphic plans and text may differ, the text takes precedence.

STATEMENT OF EXEMPTION

FILE NO. PDC09-023, PD09-020, and associated permits

LOCATION OF PROPERTY North side of Auzerais Avenue between Bird Avenue and Royal Avenue (395 Bird Avenue)

PROJECT DESCRIPTION Planned Development Rezoning from LI Light Industrial to A(PD) Planned Development, and Planned Development Permit to demolish an existing gas station and construct a new gas station, foodmart, and car wash on a 0.6 gross acre site

ASSESSOR'S PARCEL NUMBER 264-20-121

CERTIFICATION

Under the provisions of Section 15302 of the State Guidelines for Implementation of the California Environmental Quality Act (CEQA) as stated below, this project is found to be exempt from the environmental review requirements of Title 21 of the San José Municipal Code, implementing the California Environmental Quality Act of 1970, as amended.

Replacement or reconstruction of existing structures and facilities where the new structure will be located on the same site as the structure replaced and will have substantially the same purpose and capacity as the structure replaced.

Joseph Horwedel, Director
Planning, Building and Code Enforcement

Date July 02, 2009


Deputy

Project Manager: Sylvia Do

TO: Jodie Clark
Planning and Building

FROM: Ebrahim Sohrabi
Public Works

**SUBJECT: FINAL RESPONSE TO
DEVELOPMENT APPLICATION**

DATE: 01/20/10

PLANNING NO.: PDC09-023
DESCRIPTION: Planned Development Rezoning from LI Light Industrial to A(PD)
Planned Development to demolish an existing gas station and construct a
new gas station/mini-mart/car wash on a 0.6 gross acre site
LOCATION: North side of Auzerais Avenue between Bird Avenue and Royal Avenue
P.W. NUMBER: 3-15696

Public Works received revised plans for the subject project on 01/05/10 and submits the following comments and requirements.

Project Conditions:

Public Works Clearance for Building Permit(s) or Map Approval: Prior to the approval of the Tract or Parcel Map (if applicable) by the Director of Public Works, or the issuance of Building permits, whichever occurs first, the applicant will be required to have satisfied all of the following Public Works conditions. The applicant is strongly advised to apply for any necessary Public Works permits prior to applying for Building permits.

1. **Public Works Development Review Fees:** Additional Public Works Review Fees are due. Prior to the project being cleared for the hearing and approval process, these fees shall be paid to the Development Services Cashier using the attached invoice(s). Additional fees due are as follows:
 - a) A separate Traffic Report review fee in the amount of \$1,424.00.
2. **Minor Improvement Permit:** The public improvements conditioned as part of this permit require the execution of a Minor Street Improvement Permit that guarantees the completion of the public improvements to the satisfaction of the Director of Public Works. This permit includes privately engineered plans, insurance, surety deposit, and engineering and inspection fees.
3. **Transportation:**
 - a) An in-house traffic analysis was performed. The proposed use generates 32 AM and 47 PM peak hour trips; after applying credits for the existing use, the net new

trips resulted in no additional new trips. The results indicate the intersection of Auzerais Avenue and Bird Avenue was not significantly impacted with the addition of the project traffic. We conclude that the subject project will be in conformance with the City of San Jose Transportation Level of Service Policy (Council Policy 5-3). Therefore, a determination of a negative declaration can be made with respect to traffic impacts.

- b) The project also conforms to Council Policy 6-10, Drive-Through Uses. The project does not exacerbate the intersection of Auzerais and Bird Avenues which is currently operating at a C+ in the AM peak hour and a C in the PM peak hour.

4. **Grading/Geology:**

- a) A grading permit is required prior to the issuance of a Public Works Clearance. The construction operation shall control the discharge of pollutants (sediments) to the storm drain system from the site. An erosion control plan may be required with the grading application.
- b) The Project site is within the State of California Seismic Hazard Zone. A geotechnical investigation report addressing the potential hazard of liquefaction must be submitted to, reviewed and approved by the City Geologist prior to issuance of a grading permit or Public Works Clearance. The investigation should be consistent with the guidelines published by the State of California (CGS Special Publication 117A) and the Southern California Earthquake Center (SCEC, 1999). A recommended depth of 50 feet should be explored and evaluated in the investigation.

5. **Stormwater Runoff Pollution Control Measures:** This project must comply with the City's Post-Construction Urban Runoff Management Policy (Policy 6-29) which requires implementation of Best Management Practices (BMPs) that include site design measures, source controls, and stormwater treatment controls to minimize stormwater pollutant discharges. Post-construction treatment control measures, shown on the project's Stormwater Control Plan, shall meet the numeric sizing design criteria specified in City Policy 6-29.

- a) The project's preliminary Stormwater Control Plan and numeric sizing calculations have been reviewed. At PD stage, submit the final Stormwater Control Plan and numeric sizing calculations.
- b) Final inspection and maintenance information on the post-construction treatment control measures must be submitted prior to issuance of a Public Works Clearance.
- c) A post construction Final Report is required by the Director of Public Works from a Civil Engineer retained by the owner to observe the installation of the BMPs and stating the all post construction storm water pollution control BMPs have been installed as indicated in the approved plans and all significant changes have been reviewed and approved in advance by the Department of Public Works.

6. **Flood: Zone D**

The project site is not within a designated Federal Emergency Management Agency (FEMA) 100-year floodplain. Flood zone D is an unstudied area where flood hazards are

undetermined, but flooding is possible. There are no City floodplain requirements for zone D.

7. **Sewage Fees:** In accordance with City Ordinance all storm sewer area fees, sanitary sewer connection fees, and sewage treatment plant connection fees, less previous credits, are due and payable.
8. **Undergrounding:**
 - a) The In Lieu Undergrounding Fee shall be paid to the City for all frontages adjacent to Auzerais and Royal Avenues prior to issuance of a Public Works clearance. One hundred percent (100 %) of the base fee in place at the time of payment will be due. (Currently, the base fee is \$395 per linear foot of frontage.)
 - b) The Director of Public Works may, at her discretion, allow the developer to perform the actual undergrounding of all off-site utility facilities fronting the project adjacent to Royal Avenue. Developer shall submit copies of executed utility agreements to Public Works prior to the issuance of a Public Works Clearance.
9. **Street Vacation:** This project may vacate up to 26' of Auzerais Avenue along the project frontage. A preliminary title report must be submitted prior to any decisions regarding the possible vacation of Auzerais Avenue. The street vacation process will require further discretionary approval by the City Council. If street vacation is desired, this project will be subject to this process prior to Public Works Clearance.
10. **Street Improvements:**
 - a) Applicant shall be responsible to remove and replace curb, gutter, and sidewalk damaged during construction of the proposed project.
 - b) Remove and replace broken or uplifted curb, gutter, and sidewalk along project frontages.
 - c) Close unused driveway cut(s).
 - d) Close the easterly driveway on Auzerais Avenue.
 - e) Upgrade handicap ramps to meet current ADA requirements by installing detectable warning features (see CSJ Standard Details R-10 or R-11) at the corners of Bird and Auzerais Avenues and Royal and Auzerais Avenues.
 - f) Construct a 4' concrete median island along Auzerais Avenue frontage to prevent left turns from the site.
 - g) Repair, overlay, or reconstruction of asphalt pavement may be required. The existing pavement will be evaluated with the street improvement plans and any necessary pavement restoration will be included as part of the final street improvement plans.
11. **SNI:** This project is located within the Burbank/Del Monte SNI area. Public improvements shall conform to the approved EIR and neighborhood improvement plan.
12. **Storm:** Identify the following on the Grading/Drainage plan at the Planned Development stage:

- a) Construct 12" RCP storm lateral to existing storm main on Bird Avenue.
- b) Relocate existing hooded inlet out of proposed driveway, construct new manhole as necessary.

13. **Electrical:**

- a) Existing electroliers along the project frontage will be evaluated at the public improvement stage and any street lighting requirements will be included on the public improvement plans
- b) Locate and protect existing electrical conduit in driveway and/or sidewalk construction.

14. **Street Trees:**

- a) The locations of the street trees will be determined at the street improvement stage. Street trees shown on this permit are conceptual only.
- b) Contact the City Arborist at (408) 277-2756 for the designated street tree.
- c) Install street trees within public right-of-way along entire project street frontage per City standards; refer to the current "Guidelines for Planning, Design, and Construction of City Streetscape Projects". Street trees shall be installed in cut-outs at the back of curb. Obtain a DOT street tree planting permit for any proposed street tree plantings.

Please contact the Project Engineer, Vivian Tom, at (408) 535-6819 if you have any questions.



Ebrahim Sohrabi
Senior Civil Engineer
Development Services Division



ES:VT:atl
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c: Stevan Nakashima, S.N.C.E.

CHEVRON GAS STATION, FOOD MART & CARWASH

395 BIRD AVENUE
SAN JOSE, CALIFORNIA 95126

FILE NO: PDC09-023

DRAWING INDEX

1	TITLE SHEET
2	LAND USE PLAN
3	CONCEPTUAL SITE PLAN
4	GRADING & DRAINAGE PLAN
4A	STORMWATER CONTROL PLAN
4B	STORMWATER CONTROL PLAN
4C	UNDERGROUND PIPING PLAN
4D	STORMWATER CONTROL O&M
4E	STORMWATER CONTROL O&M
5	CONCEPTUAL BUILDING ELEVATIONS
6	CONCEPTUAL FLOOR PLAN
7	CONCEPTUAL LANDSCAPE PLAN

SITE INFO

APN: 264-20-121
 JURISDICTION: CITY OF SAN JOSE, CA
 CURRENT ZONING: LI (LIGHT INDUSTRIAL DISTRICT)
 REZONING: A (PD) PLANNED DEVELOPMENT DISTRICT

MINIMUM LOT AREA: 6000 SF.

SEPARATES BUILDING AND/OR PARKING & CIRCULATIONAL

ADJACENT TO STREETS:

6 FEET (AUZERAIS)
6 FEET (BIRD)
6 FEET (ROYAL)

REAR SIDE: 0 FEET

BUILDING HEIGHT (MAX.): THE MAXIMUM BUILDING HEIGHT SHALL NOT EXCEED FOURTY-FIVE FEET (45'), AND NO MORE THAN TWO (2) STORIES SHALL BE PERMITTED.

PARKING REQUIREMENTS:

FOOD MART: 1 SPACE PER 200 SF. : 2,070 SF. x 0.05 / 200 = 0.5

GAS STATION: 1 SPACE PER EMPLOYEE (8 EMPLOYEES) = 1
 1 SPACE PER AIR/WATER = 1
 1 SPACE FOR INFORMATION STOP = 1 (OVERLAP W FOOD MART)

CARWASH: VEHICLE STACKING FOR (3) CARS

PRIVATE OFFICE: 1 SPACE PER 250 SF. : 550 SF. x 0.05 / 250 = 1.1

PARKING PROVIDED:

13 SPACES
13 SPACES
10 SPACES
2 SPACES
1 SPACE

SITE COVERAGES:

SITE: 26,000 SF. - 60 ACRES (0.00%)
EXISTING BUILDINGS: 9,423 SF. (0.36%)
AUTO SERVICE / SNACK SHOP, FUEL CANOPY: 2,242 SF. (0.01%)
EXISTING LANDSCAPING: 3,544 SF. (0.14%)
EXISTING IMPERVIOUS AREA: 14,086 SF. (0.54%)
NEW BUILDINGS: 5,424 SF. (0.21%)
FOOD MART: 2,070 SF. (0.08%)
CARWASH TUNNEL/EQUIP. RM.: 1,146 SF. (0.04%)
PRIVATE OFFICE (2ND FLR): 550 SF. (0.02%)
FUELING CANOPY: 2,150 SF. (0.08%)
NEW LANDSCAPING: 4,461 SF. (0.17%)
POST CONSTRUCTION IMPERVIOUS AREA: 15,642 SF. (0.61%)



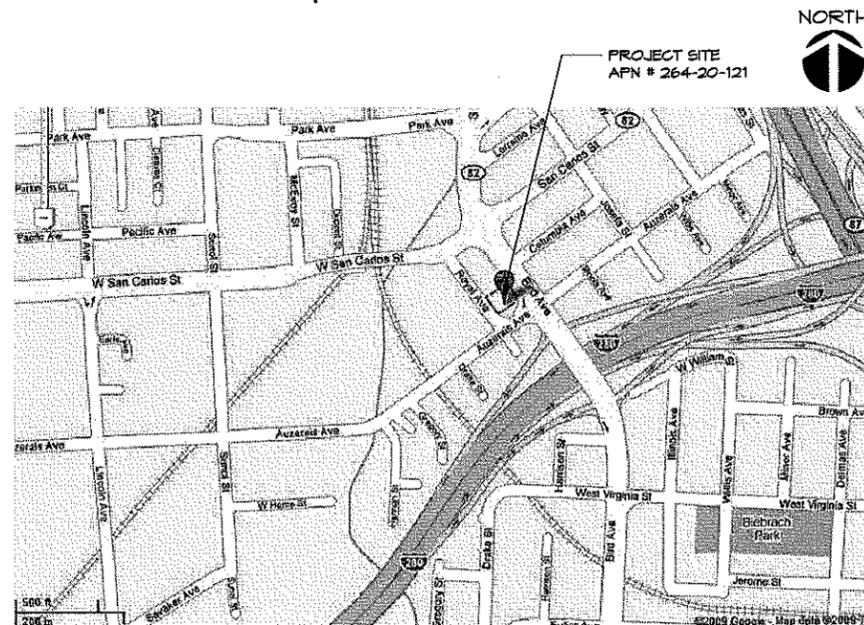
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CHEVRON GAS STATION
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LOCATION MAP SCALE: 1" = 500'



PROJECT DESCRIPTION

- REZONE THE PROPERTY FROM THE LI- LIGHT INDUSTRIAL ZONING DISTRICT TO THE A (PD)- PLANNED DEVELOPMENT ZONING DISTRICT
- REMOVE (E) AUTO SERVICE / SNACK SHOP BUILDING
- REMOVE (E) AIR/WATER STATION & TELEPHONE BOOTH
- REMOVE (E) LANDSCAPE PLANTERS ALONG AUZERAS AVE. & ROYAL AVE. & AT CORNER OF ROYAL AVE. & AUZERAS AVE.
- REMOVE (E) DRIVEWAY ON ROYAL AVE. & TWO (E) DRIVEWAYS ON AUZERAS AVE.
- REMOVE A PORTION OF THE LANDSCAPE PLANTER ALONG BIRD AVE. TO ACCOMMODATE NEW DRIVEWAY LOCATION
- EXISTING FUELING CANOPY COVERING (4) EXISTING DISPENSERS TO REMAIN
- EXISTING UNDERGROUND STORAGE TANKS TO REMAIN (TYP. OF 4)
- EXISTING MONUMENT SIGN TO REMAIN
- CONSTRUCT A 2,070 S.F. FOOD MART BUILDING W/ ATTACHED 1,146 S.F. CARWASH TUNNEL/EQUIPMENT ROOM ON THE FIRST FLOOR & A 550 S.F. PRIVATE OFFICE ON THE SECOND FLOOR
- CONSTRUCT A 10'-0" W x 15'-4" D x 6'-0" COVERED MASONRY TRASH ENCLOSURE W/ SOLID METAL GATES
- INSTALL NEW AIR/WATER/VACUUM ISLAND
- INSTALL NEW AREA LIGHTS
- INSTALL NEW PARKING STALLS, PER CITY STANDARDS
- INSTALL NEW LANDSCAPING, PER CITY STANDARDS

PROJECT INFO

FOOD MART (2,070 S.F.):	M OCCUPANCY, TYPE VB
OFFICE (550 S.F.):	B OCCUPANCY, TYPE VB
CARWASH TUNNEL/EQUIP. RM. (1,146 S.F.):	B OCCUPANCY, TYPE VB
FUELING CANOPY (2,150 S.F.):	M OCCUPANCY, TYPE IIB

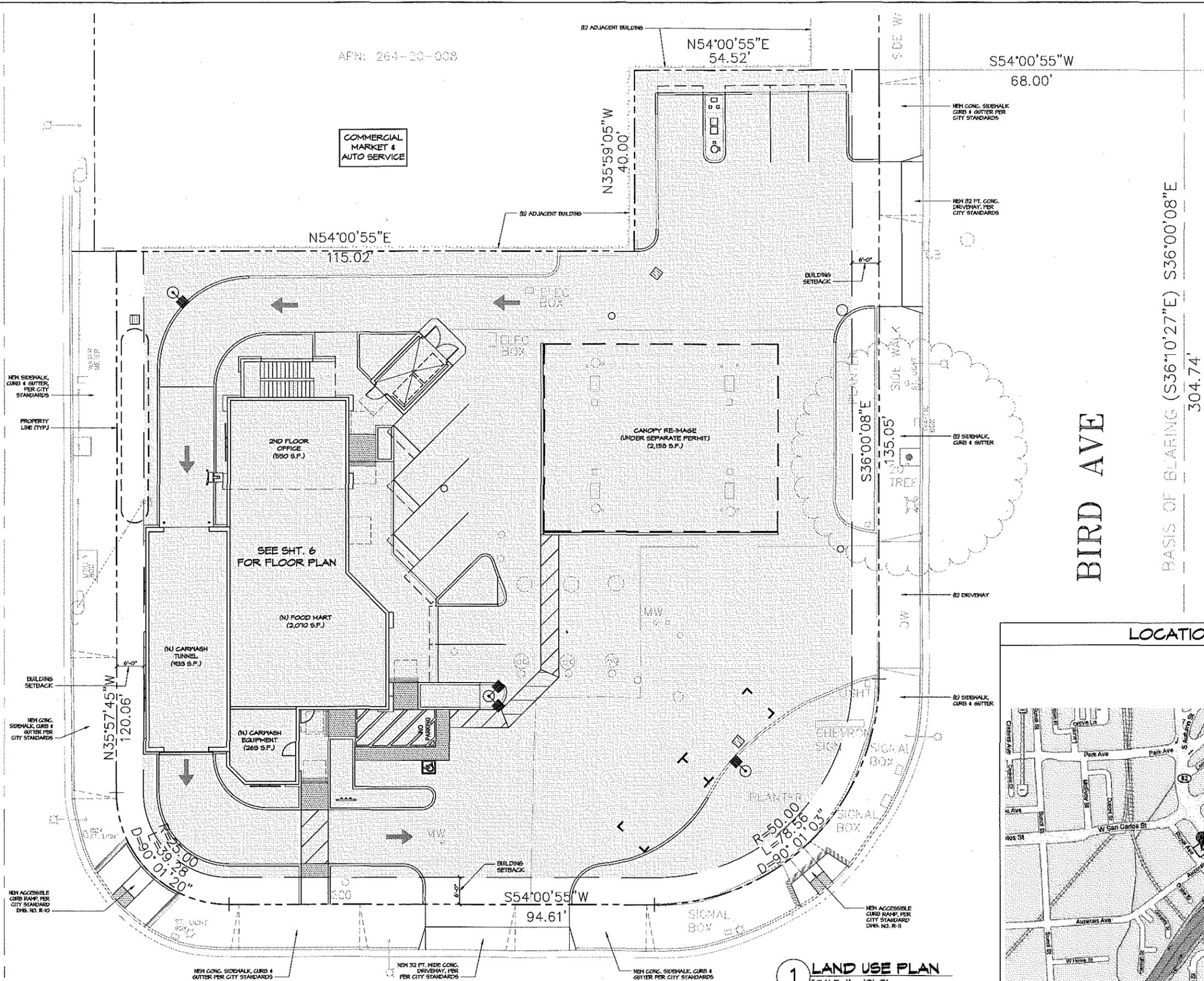
PROJECT TEAM

ARCHITECT M I ARCHITECTS, INC. 2960 CAMINO DIABLO, SUITE 100 WALNUT CREEK, CA 94591 TEL: (425) 818-4815 FAX: (425) 943-1581 CELL: (425) 818-4875 MR. MITHANA IBRAHIM, ARCHITECT	CIVIL ENGINEER S.N.C.E. 1420 HOLLY AVENUE LOS ALTOS, CA 94024 TEL: (650) 964-4211 FAX: (650) 964-4224 MR. STEVAN H. NAKASHIMA
LANDSCAPE ARCHITECT GIARDIELLA ASSOCIATES 951 ROSE AVENUE MENLO PARK, CA 94025 TEL: (650) 326-6100 FAX: (650) 323-6106 CELL: (415) 651-4900 MR. RICHARD GIARDIELLA	DEVELOPER VINTNERS DISTRIBUTORS, INC 4100S ALBIRAE STREET, 2ND FLOOR FREMONT, CA 94538 TEL: (510) 651-4950 FAX: (510) 651-4900 MR. SANNI GOYAL

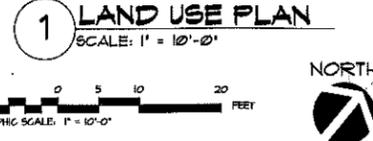
ISSUED FOR CONSTRUCTION
ISSUED FOR PLAN CHECK
08-08-01 ISSUED FOR PLANNING
NO. DATE DESCRIPTION
12-10-01 REVISION PERFORM WATER COMMENTS
1
COVER SHEET
PROJECT #: 08-5001
DRAWN: JM CHECKED: MH
SCALE: AS NOTED DATE: 08-08-01
GENERAL DEVELOPMENT PLAN-EXHIBIT C
1
SHEET OF

S:\1-Projects\08-5001_RI_Remodel San Jose\Drawings\08-5001-501_3.dwg modified by mskdtraher at Feb 02, 2010 - 11:24am

ROYAL AVE



AUZERAIS AVE



1 LAND USE PLAN
SCALE: 1" = 10'-0"

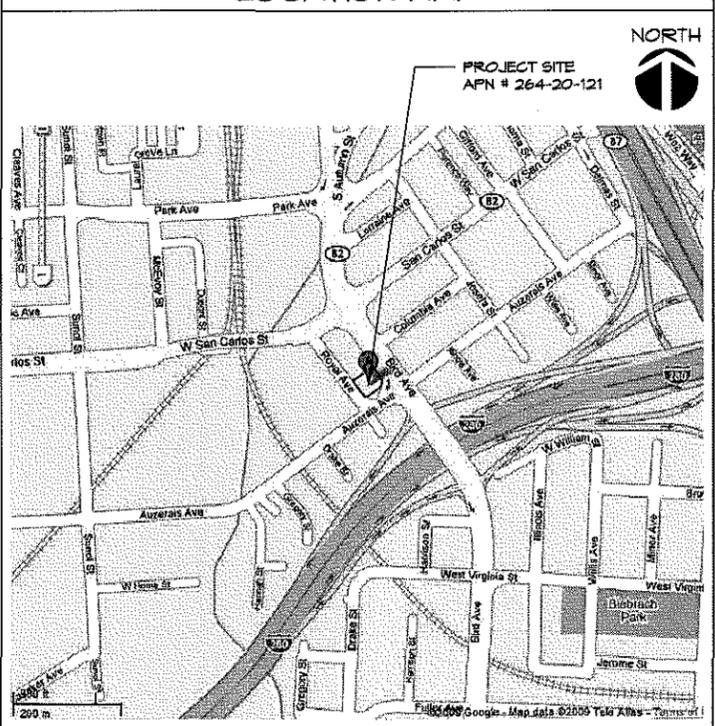
SITE INFO/DEVELOPMENT STANDARDS

APN	264-20-121
JURISDICTION	CITY OF SAN JOSE, CA
CURRENT ZONING	LI (LIGHT INDUSTRIAL DISTRICT)
REZONING	A (PD) PLANNED DEVELOPMENT DISTRICT
MINIMUM LOT AREA	6,000 SF.
SETBACKS (BUILDING AND/OR PARKING & CIRCULATION)	
ADJACENT TO STREETS	6 FEET (AUZERAS)
	6 FEET (BIRD)
	6 FEET (ROYAL)
REAR SIDE	0 FEET
	0 FEET
BUILDING HEIGHT (MAX.)	THE MAXIMUM BUILDING HEIGHT SHALL NOT EXCEED FORTY-FIVE FEET (45'), AND NO MORE THAN TWO (2) STORIES SHALL BE PERMITTED.
PARKING REQUIREMENTS:	
EQUIP. SPACE	2,070 SF. x 0.85 / 200 = 8.8 1 SPACE PER 200 SF.
GAS STATION	1 SPACE PER EMPLOYEE (1 EMPLOYEE) = 1
	1 SPACE PER AIRWATER = 1
	1 SPACE FOR INFORMATION STOP = 1 (OVERLAP W/ FOOD MART)
CARWASH	VEHICLE STACKING FOR (3) CARS
PRIVATE OFFICE	590 SF. x 0.85 / 250 = 1.9 1 SPACE PER 250 SF.
PARKING PROVIDED:	13 SPACES
PARKING REQUIRED:	13 SPACES
MIN. SIZE PARKING STALLS (8'x11')	10 SPACES
UNSIZE PARALLEL PARKING STALLS (8'x20')	2 SPACES
VAN ACCESSIBLE PARKING STALL (8'x18')	1 SPACE
SITE COVERAGES:	
SITE	26,103 SF. 60 ACRES (100%)
EXISTING BUILDINGS:	3,423 SF. (13.1%)
AUTO SERVICE / SNACK SHOP / FUEL CANOPY:	2,242 SF. (8.6%)
EXISTING LANDSCAPING:	3,544 SF. (13.6%)
EXISTING IMPERVIOUS AREA:	4,086 SF. (15.7%)
NEW BUILDINGS:	5,424 SF. (20.8%)
FOOD MART:	2,070 SF.
CARWASH TUNNEL/EQUIP. RM.:	196 SF.
PRIVATE OFFICE (2ND FLR.):	590 SF.
FUELING CANOPY:	250 SF.
NEW LANDSCAPING:	4,487 SF. (17.2%)
POST CONSTRUCTION IMPERVIOUS AREA:	15,642 SF. (60.0%)

PERMITTED USES

ALL PERMITTED USES AS ALLOWED IN THE CIG - COMBINED INDUSTRIAL / COMMERCIAL ZONING DISTRICT PER TITLE 20 OF THE SAN JOSE MUNICIPAL CODE AS AMENDED. CONDITIONAL USE AND SPECIAL USES OF THE CIG - COMBINED INDUSTRIAL / COMMERCIAL ZONING DISTRICT MAY BE CONSIDERED THROUGH THE PLANNED DEVELOPMENT PERMIT OR AMENDMENT PROCESS.

LOCATION MAP

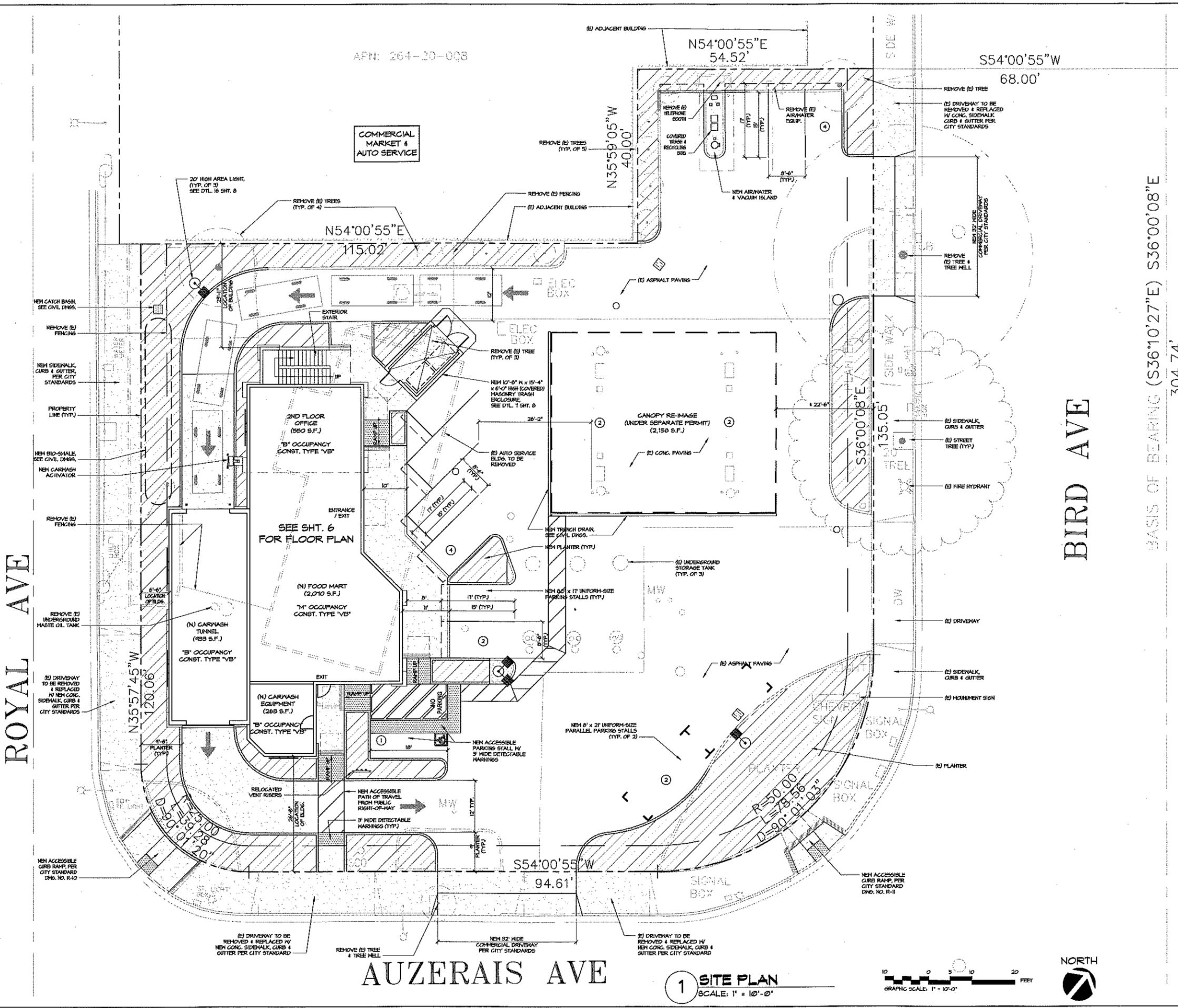


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ISSUED FOR CONSTRUCTION	
ISSUED FOR PLAN CHECK	
06-09-01 ISSUED FOR PLANNING	
NO. DATE DESCRIPTION	
12-10-01 REVISED PER STORMWATER COMMENTS	
LAND USE PLAN	
PROJECT #	08-5001
DRAWN BY	JM
CHECKED BY	MH
SCALE	AS NOTED
DATE	08-09-08
GENERAL DEVELOPMENT PLAN EXHIBIT C	

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SITE INFO

APN: 264-20-121
 JURISDICTION: CITY OF SAN JOSE, CA
 CURRENT ZONING: LI (LIGHT INDUSTRIAL DISTRICT)
 RECORDING: A (P) (PLANNED DEVELOPMENT DISTRICT)

MINIMUM LOT AREA: 6000 SF.

SETBACKS BUILDINGS AND/OR PARKING & REGULATION:

ADJACENT TO STREETS:	6 FEET (AUZERAS)
REAR SIDE:	6 FEET (BIRD)
REAR SIDE:	6 FEET (ROYAL)
REAR SIDE:	0 FEET

BUILDING HEIGHT MAX: THE MAXIMUM BUILDING HEIGHT SHALL NOT EXCEED FORTY-FIVE FEET (45'), AND NO MORE THAN TWO (2) STOREYS WILL BE PERMITTED.

PARKING REQUIREMENTS:

FOOD MART: 2070 SF. x 0.05 / 200 = 0.51 1 SPACE PER 200 SF.

GAS STATION:

1 SPACE PER EMPLOYEE (1 EMPLOYEE) = 1
 1 SPACE PER AIRMATEL = 1
 1 SPACE FOR INFORMATION STOP = 1 (OVERLAP W FOOD MART)

CARWASH:

VEHICLE STACKING FOR (5) CARS

PRIVATE OFFICE: 550 SF. x 0.05 / 250 = 1.1 1 SPACE PER 250 SF.

PARKING REQUIRED: 13 SPACES

PARKING PROVIDED: 13 SPACES

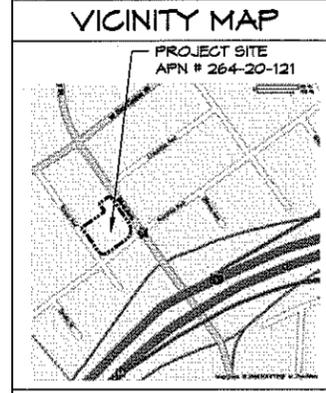
IN-SIZE PARKING STALLS (8.5X11): 10 SPACES

UNSIZE PARALLEL PARKING STALLS (8' x 20): 2 SPACES

VAN ACCESSIBLE PARKING STALL (7X18): 1 SPACE

SITE COVERAGES:

SITE:	26,003 SF. (60.2%)
EXISTING BUILDINGS:	3,423 SF. (13.2%)
AUTO SERVICE / SNACK SHOP, FUEL CANOPY:	2,242 SF. (8.6%)
EXISTING LANDSCAPING:	3,514 SF. (13.5%)
EXISTING IMPERVIOUS AREA:	14,026 SF. (53.9%)
NEW BUILDINGS:	5,424 SF. (20.8%)
FOOD MART:	2,070 SF.
CARWASH TUNNEL/EQUIP. RM:	1,195 SF.
PRIVATE OFFICE (2ND FLR):	550 SF.
FUELING CANOPY:	2,509 SF.
NEW LANDSCAPING:	4,987 SF. (19.2%)
POST CONSTRUCTION IMPERVIOUS AREA:	15,612 SF. (60.2%)



SITE PLAN LEGEND

	LANDSCAPING
	NEW BIO-SWALE, SEE SHT. 4 & 4a
	NEW CONCRETE PAVING
	NEW CURB
	EXISTING CURB TO REMAIN
	EXISTING TO REMAIN
	EXISTING TO BE REMOVED
	EXISTING CURB TO BE REMOVED

CONCEPTUAL SITE PLAN

PROJECT #: 08-5001
 DRAWN: JM CHECKED: MI
 SCALE: AS NOTED DATE: 08-08-08
 GENERAL DEVELOPMENT PLAN-EXHIBIT C

Architects

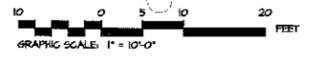
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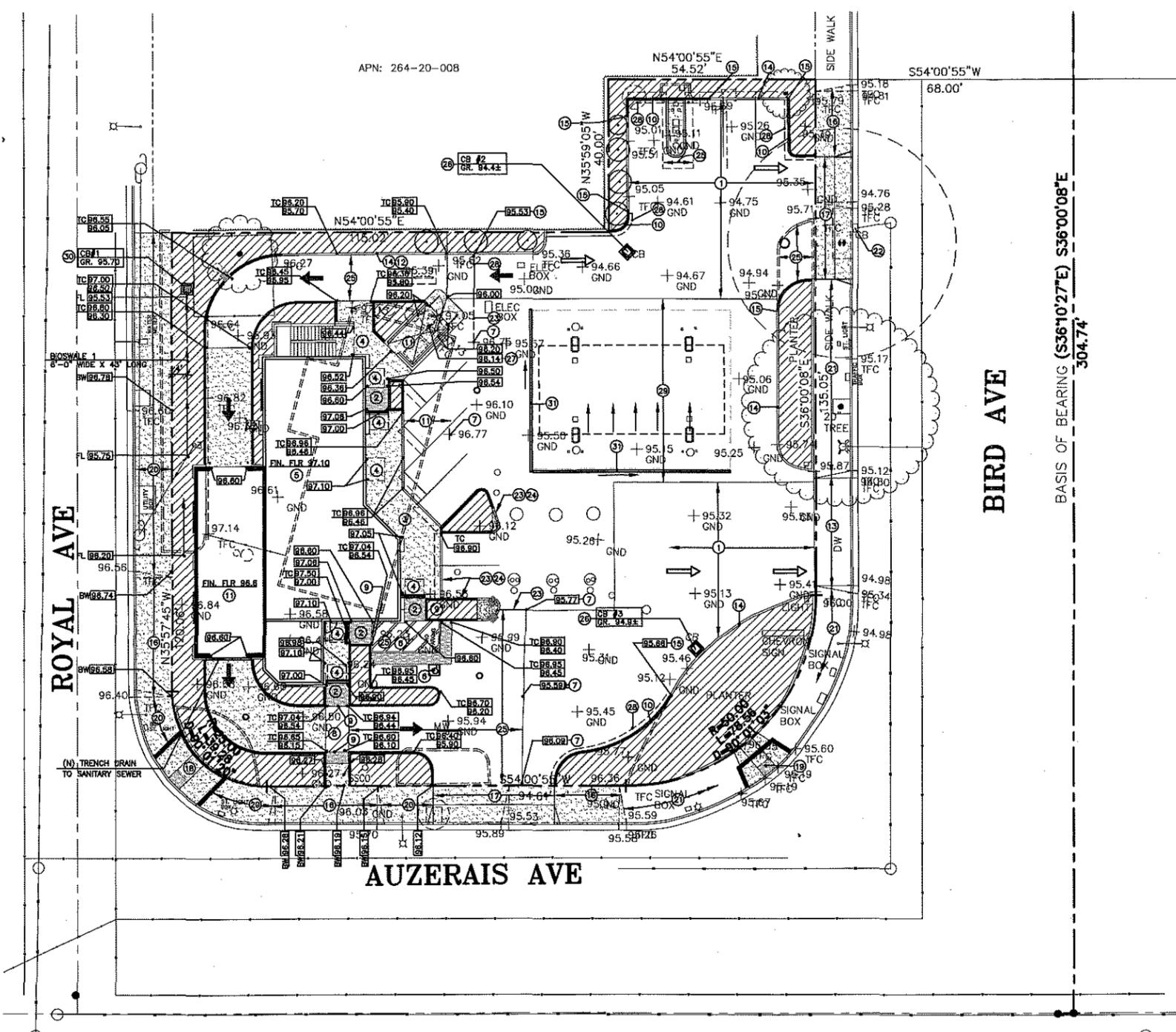
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1 SITE PLAN
SCALE: 1" = 10'-0"



Stevan Nakashima, CONSULTING CIVIL ENGINEER 12/10/2009 12:15 PM 08501B.dwg

APN: 264-20-008



PROPOSED	EXISTING	
		PROPERTY LINE
		SPOT ELEVATION
		SURFACE CONTOUR
		TOP OF CURB
		EDGE OF PAVEMENT
		FLOW LINE
		SURFACE VALLEY
		SURFACE RIDGE
		MATCH EXISTING GRADE
		CONCRETE
		CURB
		STORM DRAIN
		SANITARY SEWER
		WATER
		CATCH BASIN
		CLEANOUT TO GRADE
		STORM MANHOLE
		SANITARY MANHOLE

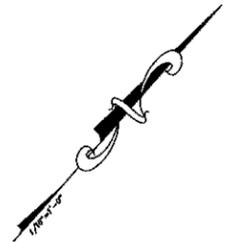
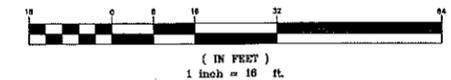
LEGEND
SCALE: NONE

KEY NOTES FOR WORK ON - SITE

- 1 (C) AC PAVING TO REMAIN.
- 2 PROVIDE CONCRETE CURB RAMP WITH MAXIMUM 1:2 SLOPE AND WITH SLOPE GREATER THAN 1:15. SEE ARCHITECTURAL DWG. FOR DETAIL.
- 3 PROVIDE NEW CONCRETE WALK WITH MAXIMUM 2% CROSS - SLOPE AND SLOPE IN THE DIRECTION OF TRAVEL OF LESS THAN 1:60.
- 4 PROVIDE CONCRETE WALK/LANDING WITH MAXIMUM 2% SLOPE IN ANY DIRECTION.
- 5 5' CONCRETE SLAB - SEE STRUCTURAL DRAWINGS FOR REINFORCEMENT - OVER 2" SAND-OVER 15 MIL MEMBRANE, OVER 4" DRAIN ROCK.
- 6 PROVIDE NEW AC PAVING WITH MAXIMUM SLOPE IN ALL DIRECTIONS OF 2% AT ALL ACCESSIBLE PARKING SPACES AND ACCESS AISLES. VERIFY LOCATION WITH ARCHITECTURAL DRAWINGS.
- 7 MATCH (C) PAVEMENT GRADE. V.I.F.
- 8 PROVIDE NEW ACCESSIBLE PATH WITH MAXIMUM 2% CROSS - SLOPE AND SLOPE IN THE DIRECTION OF TRAVEL OF LESS THAN 1:60.
- 9 PROVIDE FLUSH CURB.
- 10 PROVIDE (N) CONCRETE CURB 6" ABOVE (C) AC PAVING UND.
- 11 PROVIDE 6" CONCRETE SLAB 1/2" 6X6X1/2" 2.9X2.9X1/2" DR #3 @18" O.C. OVER 6" CL2 AGGREGATE BASE.
- 12 PROVIDE AC PAVING 6" BELOW EXISTING TOP OF CURB.
- 13 EXISTING CONCRETE DRIVE APRON TO REMAIN.
- 14 EXISTING CONCRETE CURB TO REMAIN.
- 15 TOP OF (N) CONCRETE CURB TO MATCH TOP OF (C) CONCRETE CURB.
- 16 REMOVE (C) CONCRETE DRIVE APRON AND PROVIDE (N) CURB, GUTTER AND SIDEWALK TO MATCH EXISTING PER CITY STANDARD.
- 17 PROVIDE (N) CONCRETE DRIVE APRON PER CITY STANDARD.
- 18 PROVIDE (N) CONCRETE CURB RAMP PER CITY STANDARD.
- 19 REMOVE (C) CURB RAMP AND PROVIDE (N) CONCRETE CURB RAMP PER CITY STANDARD.
- 20 REMOVE (C) CONCRETE SIDEWALK AND PROVIDE (N) SIDEWALK TO MATCH EXISTING PER CITY STANDARD.
- 21 (C) SIDEWALK TO REMAIN.
- 22 RELOCATE (C) CURB INLET PER CITY STANDARDS.
- 23 SAWCUT (C) CONCRETE PAVING AND REMOVE AS REQUIRED FOR (N) CONSTRUCTION.
- 24 PROVIDE (N) CONCRETE CURB 6" ABOVE (C) CONCRETE PAVING UND.
- 25 NEW AC PAVING. SEE PAVING TABLE.
- 26 REMOVE (C) CATCH BASIN AND PROVIDE NEW CATCH BASIN. MATCH (C) GRADE ELEVATION. V.I.F. CAP (C) STORM DRAINS.
- 27 PROVIDE FLOOR DRAIN WITH TRAP. CONNECT TO SANITARY SEWER.
- 28 SAWCUT (C) AC PAVING AND REMOVE AS REQUIRED FOR (N) CONSTRUCTION.
- 29 (C) CONCRETE PAVING TO REMAIN.
- 30 PROVIDE CATCH BASIN GRATE 2" ABOVE FLOW LINE OF BIOSWALE.
- 31 SAWCUT AND REMOVE EXISTING CONCRETE SLAB AT EDGE OF CANOPY AS REQUIRED FOR NEW TRENCH DRAIN. PROVIDE TRENCH DRAIN TO INTERCEPT RUNOFF FROM ENTERING SERVICE ISLAND AREA.

GRADING AND DRAINAGE PLAN

SCALE: 1/16" = 1'-0" GRAPHIC SCALE



NORTH

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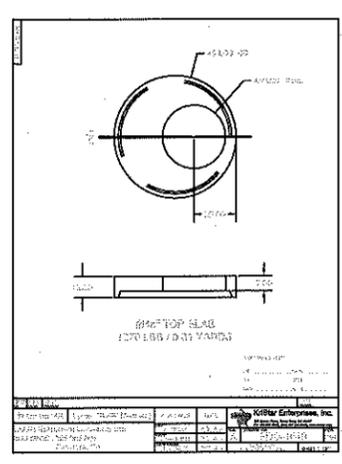
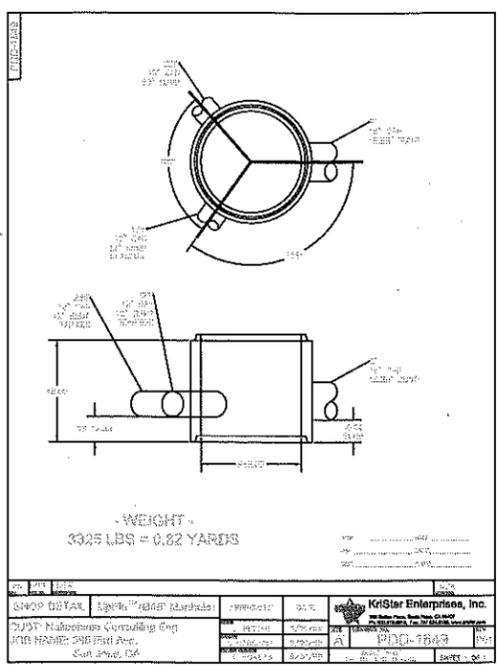
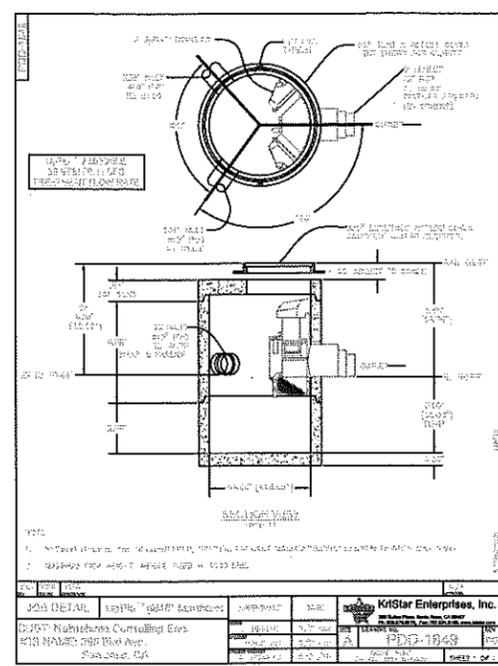


NO.	DATE	DESCRIPTION
		ISSUED FOR CONSTRUCTION
		ISSUED FOR PLAN CHECK
08-09-01		ISSUED FOR PLANNING
07-08-01		REV. PER PLANNING COMMENTS
8-8-01		REV. PER PLANNING COMMENTS
04-01-01		PER SEWER/WATER COMMENTS

GRADING AND DRAINAGE PLAN

PROJECT #: 08-3001/0850
DRAWN: FN CHECKED: SN
SCALE: AS SHOWN DATE: 07-25-09
GENERAL DEVELOPMENT PLAN SHEET C

08507B.dwg 12/07/2009 12:15 PM J-CIVIL-ENGINEER Stevan Nakashima, CONSULT.



BIOSWALE CALCULATIONS

TRIBUTARY AREA (AC)	SWALE 1	
C	.90	
I (IN/HR)	.20	
Q=CIA (CFS)	.004	
L (LENGTH OF SWALE IN FT)	43	
T (IN SECONDS)	420	
V=L/T	.10	
S	.005	
S ^{1/2}	.071	
N	.20	
R ^{1/2} = QN / (1.49AS ^{1/2}) = VN / (1.49S ^{1/2})	.19	
R	.083	
D (DEPTH IN INCHES)	1.1"	
B=2'		
A	.209	
Pw	2.58	
R	.081	

KRISTAR UPFLO MANHOLE DESIGN

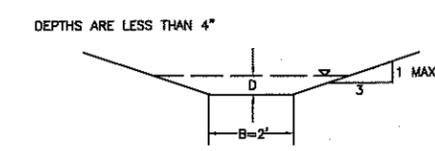
DRAINAGE AREA, AD	0.48 AC
RUNOFF COEFFICIENT, RC	0.90
WATER QUALITY FLOW	0.0864 CFS
WATER QUALITY FLOW	39 GPM
ESTIMATED PEAK STORM FLOW	0.87 CFS

FILTER SYSTEM

FILTRATION BRAND	KRISTAR
FLOW RATE PER MODULE	25.0 GPM

SUMMARY

NUMBER OF MODULES	2
MODEL NUMBER	UP-FLO MH



STRUCTURAL CONTROL MEASURES

- A. ILLEGAL DUMPING TO STORM DRAIN INLET AND WATERWAYS**
- ON-SITE STORM DRAIN SHALL BE CLEARLY MARKED WITH THE WORDS "NO DUMPING! FLOWS TO BAY." STORM WATER STENCILING, ALL DRAIN SHALL BE LABELED "NO DUMPING-FLOWS TO BAY." PLEASE CONTACT TO CITY OF SAN JOSE PUBLIC WORKS DEPARTMENT AT (408) 945-3000 TO OBTAIN FREE STENCILS.
 - IT IS UNLAWFUL TO DISCHARGE ANY WASTEWATER INTO STORM DRAINS, GUTTERS, CREEKS, OR THE SAN FRANCISCO BAY. UNLAWFUL DISCHARGES TO STORM DRAINS INCLUDE, BUT ARE NOT LIMITED TO, DISCHARGES FROM TOILETS; SINKS; INDUSTRIAL PROCESSES; COOLING SYSTEMS; BOILERS; FABRIC CLEANING; EQUIPMENT CLEANING; OR VEHICLE CLEANING.
 - IT IS UNLAWFUL TO CAUSE HAZARDOUS DOMESTIC WASTE MATERIALS TO BE DEPOSITED IN SUCH A MANNER OR LOCATION AS TO CONSTITUTE A TREATED DISCHARGE INTO STORM DRAINS, GUTTERS, CREEKS OR SAN FRANCISCO BAY.
- B. INTERIOR FLOOR DRAINS**
- INTERIOR FLOOR DRAINS SHALL BE PLUMBED TO THE SANITARY SEWER SYSTEM AND SHALL NOT BE CONNECTED TO STORM DRAINS.
- C. PESTICIDE/FERTILIZER APPLICATION**
- LANDSCAPING SHALL BE DESIGNED TO MINIMIZE IRRIGATION AND RUNOFF, PROMOTE SURFACE INFILTRATION WHERE APPROPRIATE, AND MINIMIZE THE USE OF FERTILIZERS AND PESTICIDES THAT CAN CONTRIBUTE TO STORMWATER POLLUTION.
 - STRUCTURES SHALL BE DESIGNED TO DISCHARGE TO DISCOURAGE THE OCCURRENCE AND ENTRY OF PESTS INTO BUILDINGS, THUS MINIMIZING THE NEED FOR PESTICIDES. FOR EXAMPLE, DUMPSTER AREAS SHOULD BE LOCATED AWAY FROM OCCUPIED BUILDINGS, AND FOUNDATION VENTS SHALL BE COVERED WITH SCREENS.
- D. REFUSE AREAS**
- NEW BUILDINGS SHALL PROVIDE A COVERED OR ENCLOSED AREA FOR DUMPSTERS AND RECYCLING CONTAINERS. THE AREA SHALL BE DESIGNED TO PREVENT WATER RUN-ON TO THE AREA AND RUNOFF FROM THE AREA.
 - AREAS AROUND TRASH ENCLOSURES, RECYCLING AREAS, AND/OR FOOD COMPACTOR ENCLOSURES SHALL NOT DISCHARGE TO THE STORM DRAIN SYSTEM. ANY DRAINS INSTALLED IN OR BENEATH DUMPSTERS, COMPACTORS, AND TALLOW BIN AREAS SERVING FOOD SERVICE FACILITIES SHALL BE CONNECTED TO A GREASE REMOVAL DEVICE PRIOR TO DISCHARGING TO THE SANITARY SEWER. THE APPLICANT SHALL CONTACT THE WATER POLLUTION CONTROL PLANT FOR SPECIFIC CONNECTION AND DISCHARGE REQUIREMENTS.
- E. OUTDOOR EQUIPMENT/MATERIALS STORAGE**
- ALL OUTDOOR EQUIPMENT AND MATERIALS STORAGE AREAS SHALL BE COVERED AND BERMED, OR SHALL BE DESIGNED TO LIMIT THE POTENTIAL FOR RUNOFF TO CONTACT POLLUTANTS OR A STORM DRAIN INLET VALVE SHALL BE PROVIDED ON EXTERIOR DRAINS IN THE AREA.
 - STORAGE AREA CONTAINING NON-HAZARDOUS LIQUIDS SHALL BE COVERED BY A ROOF AND/OR DRAIN TO THE SANITARY SEWER SYSTEM, AND BE CONTAINED BY BERMS, DIKES, LINERS OR VAULTS. THE APPLICANT SHALL CONTACT THE WATER POLLUTION CONTROL PLANT FOR SPECIFIC CONNECTION AND DISCHARGE REQUIREMENTS.
 - ANY HAZARDOUS MATERIALS REGULATED BY CHAPTER 17.68 OF THE SAN JOSE MUNICIPAL CODE ON THE SITE MUST BE USED AND STORED IN FULL COMPLIANCE WITH THE CITY'S HAZARDOUS MATERIAL ORDINANCE AND THE HAZARDOUS MATERIALS MANAGEMENT PLAN FOR THE SITE APPROVED BY THE SAN JOSE FIRE PREVENTION BUREAU.
- F. VEHICLE/EQUIPMENT CLEANING**
- WASTEWATER FROM VEHICLE AND EQUIPMENT WASHING OPERATIONS SHALL NOT BE DISCHARGED TO THE STORM DRAIN SYSTEM.
 - COMMERCIAL CAR WASH FACILITIES SHALL BE DISCHARGED AND OPERATED SUCH THAT NO RUNOFF FROM THE FACILITIES IS DISCHARGED TO THE STORM DRAIN SYSTEM. WASTEWATER FROM THE FACILITY SHALL DISCHARGE TO THE SANITARY SEWER OR A WASTEWATER RECLAMATION SYSTEM SHALL BE INSTALLED. THE APPLICANT SHALL CONTACT THE WATER POLLUTION CONTROL PLANT FOR SPECIFIC CONNECTION AND DISCHARGE REQUIREMENTS.
- G. FUEL DISPENSING AREAS**
- FUELLING AREAS SHALL HAVE IMPERMEABLE FLOORS (I.E., PORTLAND CEMENT CONCRETE OR EQUIVALENT SMOOTH IMPERVIOUS SURFACE) THAT ARE: A) GRADED AT THE MINIMUM SLOPE NECESSARY TO PREVENT PONDING; AND B) SEPARATED FROM THE REST OF THE SITE BY A GRADE BREAK THAT PREVENTS RUN-ON OF STORMWATER TO THE MAXIMUM EXTENT PRACTICABLE.
 - FUELLING AREAS SHALL BE COVERED BY A CANOPY THAT EXTENDS A MINIMUM OF TEN FEET IN EACH DIRECTION FROM EACH PUMP. ALTERNATIVELY, THE FUELING AREA MUST BE COVERED AND THE COVER'S MINIMUM DIMENSIONS MUST BE EQUAL TO OR GREATER THAN THE AREA WITHIN THE GRADE BREAK OR FUEL DISPENSING AREA, AS DEFINED BELOW. THE CANOPY OR COVERED SHALL NOT DRAIN ONTO THE FUELING AREA.
- H. FIRE SPRINKLER TEST WATER**
- SANITARY SEWER CONNECTIONS SHALL BE PROVIDED TO DRAIN FIRE SPRINKLER TEST WATER.
- I. MISCELLANEOUS DRAIN OR WASH WATER**
- AIR COMPRESSOR OR AIR CONDITIONER CONDENSATE DRAIN LINES MAY NOT DISCHARGE TO THE STORM DRAIN SYSTEM.
 - ROOF DRAINS SHALL DISCHARGE AND DRAIN AWAY FROM THE BUILDING FOUNDATION TO AN UNPAVED AREA WHEREVER POSSIBLE.
 - ROOF TOP EQUIPMENT SHALL DRAIN TO THE SANITARY SEWER. THE APPLICANT SHALL CONTACT THE WATER POLLUTION CONTROL PLANT FOR SPECIFIC CONNECTION AND DISCHARGE REQUIREMENTS.
- OPERATIONAL BMPs**
- A. PAVED SIDEWALKS AND PARKING LOTS**
- SIDEWALKS AND PARKING LOTS SHALL BE SWEEP REGULARLY TO PREVENT THE ACCUMULATION OF LITTER AND DEBRIS. DEBRIS RESULTING FROM PRESSURE WASHING SHALL BE TRAPPED AND COLLECTED TO PREVENT ENTRY INTO THE STORM DRAIN SYSTEM. WASHWATER CONTAINING ANY CLEANING AGENT OR DEGREASER SHALL BE COLLECTED AND DISCHARGED TO THE SANITARY SEWER AND SHALL NOT BE DISCHARGED TO A STORM DRAIN. THE APPLICANT SHALL CONTACT THE WATER POLLUTION CONTROL PLANT FOR SPECIFIC CONNECTION AND DISCHARGE REQUIREMENTS.
- B. PRIVATE STREETS**
- OWNER PRIVATE STREETS AND STORM DRAINS SHALL PREPARE AND IMPLEMENT A PLAN FOR STREET SWEEPING OF PAVED PRIVATE ROADS AND CLEANING OF ALL STORM DRAIN INLETS.
- C. FUELING AREAS**
- THE PROPERTY OWNER SHALL DRY SWEEP THE FUELING AREA ROUTINELY.
 - THE PROPERTY OWNER SHALL PREPARE A SPILL CLEANUP PLAN IN CONFORMANCE WITH THE CITY OF SAN JOSE FIRE CODE.

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08-08-09 ISSUED FOR PLANNING
NO. DATE DESCRIPTION
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10-09-09 REV. PER PLANNING COMMENTS
12-09-09 REV. PER STORMWATER COMMENTS

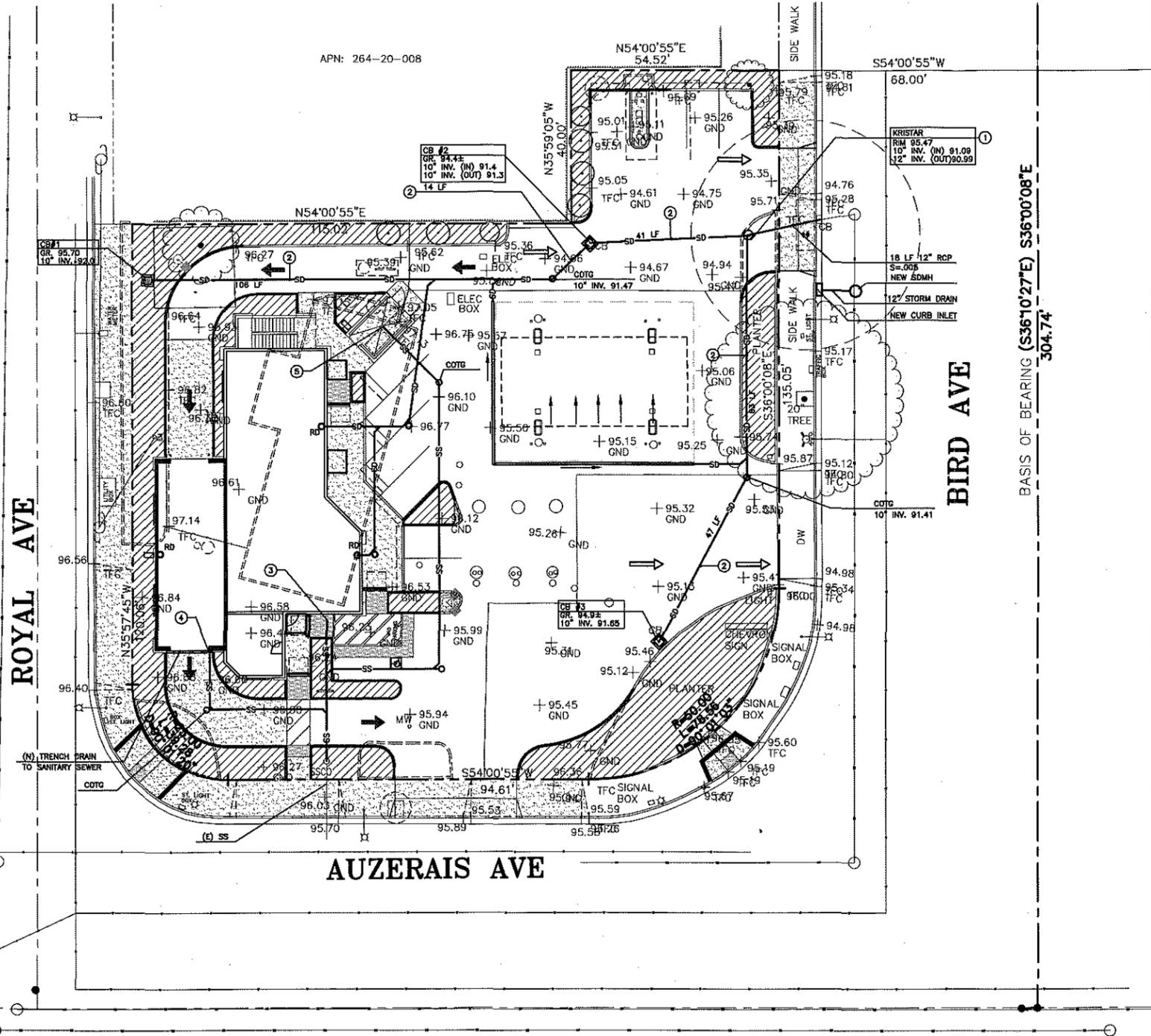
STORMWATER CONTROL PLAN

PROJECT #: 08-5001/0850
DRAWN: FN CHECKED: SN
SCALE: AS SHOWN DATE: 07-25-09
GENERAL DEVELOPMENT PLAN - EXHIBIT C

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NOT FOR CONSTRUCTION

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REGISTERED PROFESSIONAL ENGINEER
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ISSUED FOR PLAN CHECK

08-05-04 ISSUED FOR PLANNING

NO. DATE DESCRIPTION

07-06-04 REV. PER PLANNING COMMENTS

11-01-04 REV. PER PLANNING COMMENTS

12-10-04 PER STORM-WATER COMMENTS

UNDERGROUND PIPING PLAN

PROJECT # 08-5001/0850

DRAWN: FN CHECKED: SN

SCALE: AS SHOWN DATE: 07-25-04

GENERAL DEVELOPMENT PLAN-EXHIBIT C

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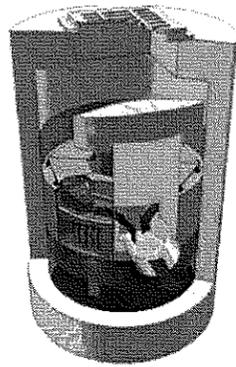
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SHEET OF



Hydro International



The Up-Flo Filter Stormwater Treatment System

Operation and Maintenance Manual

UP-FLO™ FILTER OPERATION AND MAINTENANCE MANUAL

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DISCLAIMER: Information and data contained in this manual is exclusively for the purpose of assisting in the operation and maintenance of Hydro International's Up-Flo™ Filter.



UP-FLO™ FILTER OPERATION AND MAINTENANCE MANUAL

Up-Flo™ Filter by Hydro International

Hydro International's Up-Flo™ Filter is the new standard in stormwater treatment.

Designed to meet the most stringent stormwater regulations by capturing a wide range of pollutants including fine sediment, oil, grease, bacteria, metals, silt and debris, the Up-Flo™ Filter is a high rate, modular system that combines a patented up-flow path and unique Drain Down design.

The multiple treatment train capabilities of the Up-Flo™ Filter - settling, screening and filtration - make it the most effective and economical stormwater treatment system available.

BENEFITS OF UP-FLO™ TECHNOLOGY

- Large filter area
• High flow capacity
• Resistant to clogging
• Self-cleans during drain-down period
• Media not submerged between events
• Media does not move or captured materials

CAPABILITIES

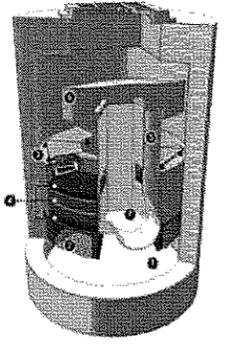
- Removes trash, sediment, silt, debris, metals and oils
• No pollutant washouts
• Wide range of treatment flows
• Drain down prevents leaching of captured pollutants
• Modular design allows for easy retrofit installations
• Small footprint
• Easy maintenance

APPLICATIONS

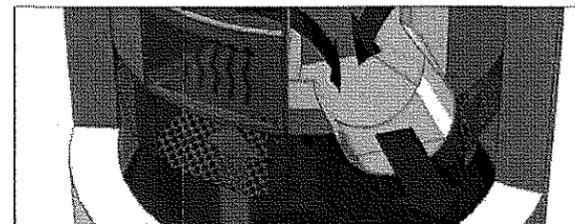
- New developments and retrofits
• Catch basins or flow-through chambers
• Streets and roadways
• Parking lots
• Vehicle maintenance wash-down yards
• Industrial and commercial facilities
• Wetlands protection
• Utility yards

UP-FLO™ FILTER COMPONENTS

- 1. Sump
2. Angled Screen
3. Filter Module
4. Media Pack
5. Outlet Module
6. Bypass Siphon with Variable Baffle
7. Drain Down Port



UP-FLO™ FILTER OPERATION AND MAINTENANCE MANUAL



Operation

INTRODUCTION

The Up-Flo™ Filter operates on simple fixed hydraulics. It is self-activating, has no moving parts, no external power requirement and is fabricated with durable non-oxidative components.

POLLUTANT CAPTURE

The Up-Flo™ Filter is designed to work as a "treatment basin" by incorporating multiple treatment technologies into a single device.

REDUCED CLOGGING

The Up-Flo™ Filter has been designed to minimize the chance of clogging and blinding. The Up-Flo™ Filter employs a unique Drain Down design that allows the water level in the chamber to drop below the filter media between events.

OVERFLOW PROTECTION

The Angled Screen is designed to minimize the chance of clogging and blinding. The Angled Screen is situated below the Filter Module, sheltering them from the direct path of the inflow.

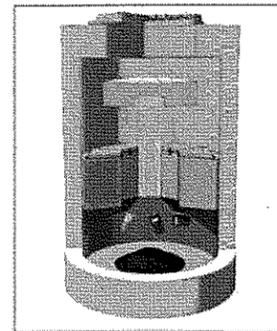


Figure 1: Pollutants captured in the Up-Flo™ Filter

UP-FLO™ FILTER OPERATION AND MAINTENANCE MANUAL

Maintenance

OVERVIEW

The Up-Flo™ Filter protects the environment by removing a wide range of pollutants from stormwater runoff. Periodic removal of these captured pollutants is essential to the proper functioning of the Up-Flo™ Filter.

The Up-Flo™ Filter design allows for easy and safe inspection, monitoring and clean-out procedures. The Up-Flo™ Filter has a wide central channel for easy and comfortable access to the Up-Flo™ Filter components (see Figure 2).

Maintenance events can be categorized as Routine or Annual. Routine maintenance can include inspection, floatables removal and sediment removal. Routine maintenance events do not require entry into the Up-Flo™ chamber. In the case of inspection and floatables removal, a vector truck is not required. However, a vector truck is required if the floatables are to be included in sediment removal from the pump of the Up-Flo™ vessel. Annual maintenance includes Media Pack replacement in addition to sediment and floatable removal. In most instances, entry into the Up-Flo™ vessel is required for Media Pack replacement. There is the need to follow OSHA Confined Space Entry procedures when performing annual maintenance.

DETERMINING YOUR MAINTENANCE SCHEDULE

The frequency of maintenance procedures can be determined in the field after installation. Kristar Enterprises, however, recommends that routine maintenance be completed at least every six months during the first year of operation.

During the first year of operation, the unit should be inspected every six months to determine the rate of sediment and floatable accumulation. To prevent from blocking the entryway to the filter media, the sediment must be removed before it completely fills the sump. A simple probe, such as the Sludge Judge®, can be used to determine the layer of solids in the sediment storage facility. This information can be recorded in maintenance logs to establish a routine maintenance schedule. Pollutant storage volume will be dependent on the sump depth of the Up-Flo™ Filter. A typical 4-ft round manhole Up-Flo™ Filter has 0.6 yd³ of sediment storage.

Routine and Annual maintenance procedures for a typical 4-ft diameter manhole Up-Flo™ Filter take less than 30 minutes and remove about 350 gallons of water in the process. Spent Media Bags weigh about 40 lbs. Spent Media Packs should be disposed of at your local landfill.

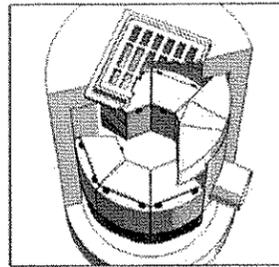


Figure 2: The wide channel in the Up-Flo™ Filter

ROUTINE MAINTENANCE PROCEDURE

INSPECTION

Inspection is a simple process that does not involve entry into the Up-Flo™ chamber. Maintenance crews should be familiar with the Up-Flo™ Filter and its components prior to inspection.

SCHEDULING

- The first year following installation, it is important to inspect your Up-Flo™ Filter regularly to determine your site specific rate of pollutant accumulation
• Typically, inspection may be conducted during any season of the year

RECOMMENDED EQUIPMENT

- Safety Equipment and Personal Protective Equipment (traffic cones, work gloves, etc.)
• Crow bar to remove grate or lid
• Pole with skimmer or net
• Sediment probe (such as a Sludge Judge®)
• Kristar Enterprises Up-Flo™ Filter Maintenance Log
• Trash bag for removed floatables
• Up-Flo™ Filter Maintenance Log

UP-FLO™ FILTER OPERATION AND MAINTENANCE MANUAL

INSPECTION PROCEDURES

- 1. Set up any necessary safety equipment such as traffic cones around the access port or grate of the Up-Flo™ Filter. Safety responses should notify passing pedestrian and road traffic that work is being done.
2. Remove the grate or lid to the manhole or vault.
3. Without entering the vessel, look down into the chamber to inspect the inside. Make note of any irregularities. See Figure 4 for a typical Inspection View.
4. Without entering the vessel, use the pole with the skimmer net to remove floatables and loose debris from the chamber.
5. Using a sediment probe such as a Sludge Judge®, measure the depth of sediment that has collected in the sump of the vessel.
6. On the Maintenance Log provided by Kristar Enterprises record the date, unit location, estimated volume of floatables and gross debris removal, and the depth of sediment measured. Also note any apparent irregularities such as damaged components or blockages.
7. Securely replace the grate or lid.
8. Take down safety equipment.
9. Contact Kristar Enterprises to discuss any irregularities noted during inspection.

FLOATABLES AND SUMP CLEANOUT

A commercially or municipally owned sump-vac is used to remove captured sediment and floatables (Figure 5).

Floatables and loose debris can also be netted with a skimmer and pole. The access port located at the top of the manhole facilitates unobstructed access for a vector truck and skimmer pole to be lowered to the bottom of the sump.

SCHEDULE

- Floatables and sump cleanout may typically be done during any season of the year
• Floatables and sump cleanout should occur as soon as possible following a spill in the contributing drainage area

RECOMMENDED EQUIPMENT

- Safety Equipment (traffic cones, etc.)
• Crow bar to remove grate or lid
• Pole with skimmer or net (if only floatables are being removed)
• Sediment probe (such as a Sludge Judge®)
• Vector truck (if vector hose preferred)
• Hose, pressure washer, or other service-cleaning device
• Kristar Enterprises Up-Flo™ Filter Maintenance Log

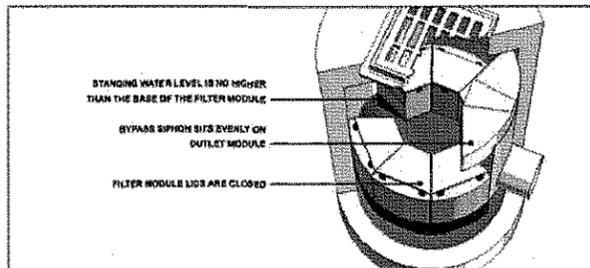


Figure 4: Inspection view of the Up-Flo™ Filter



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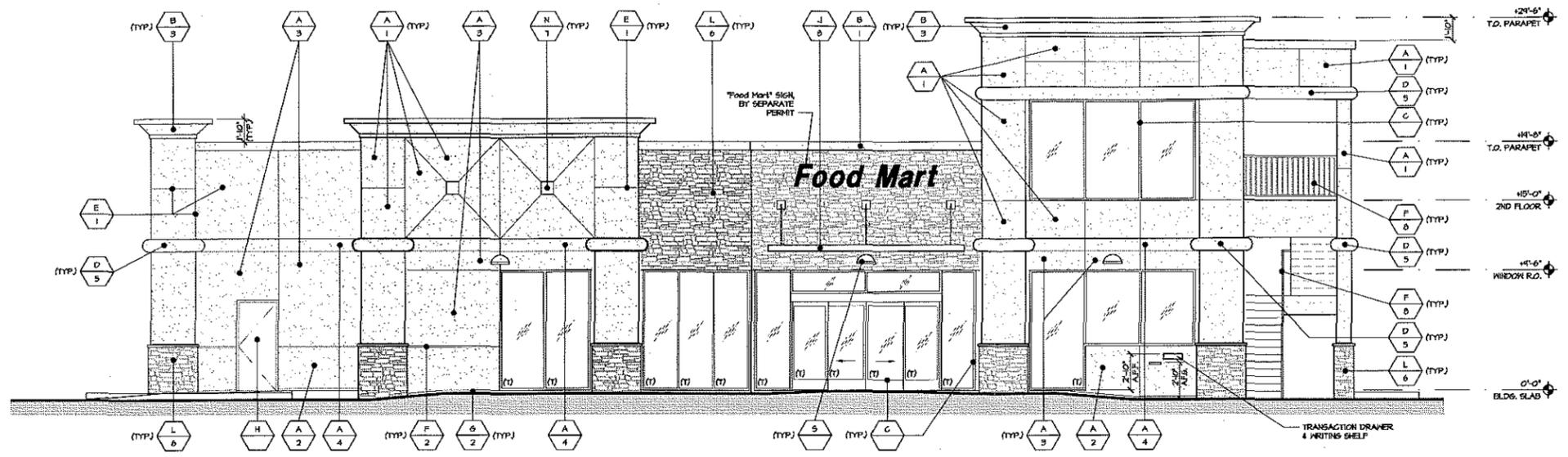
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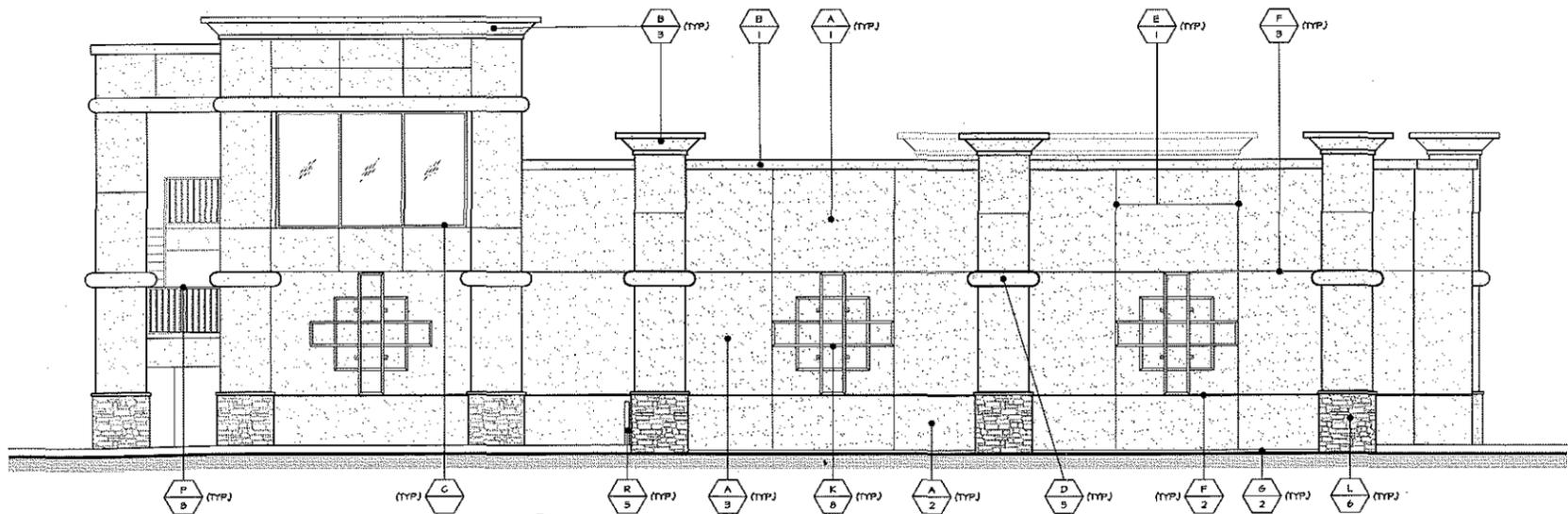
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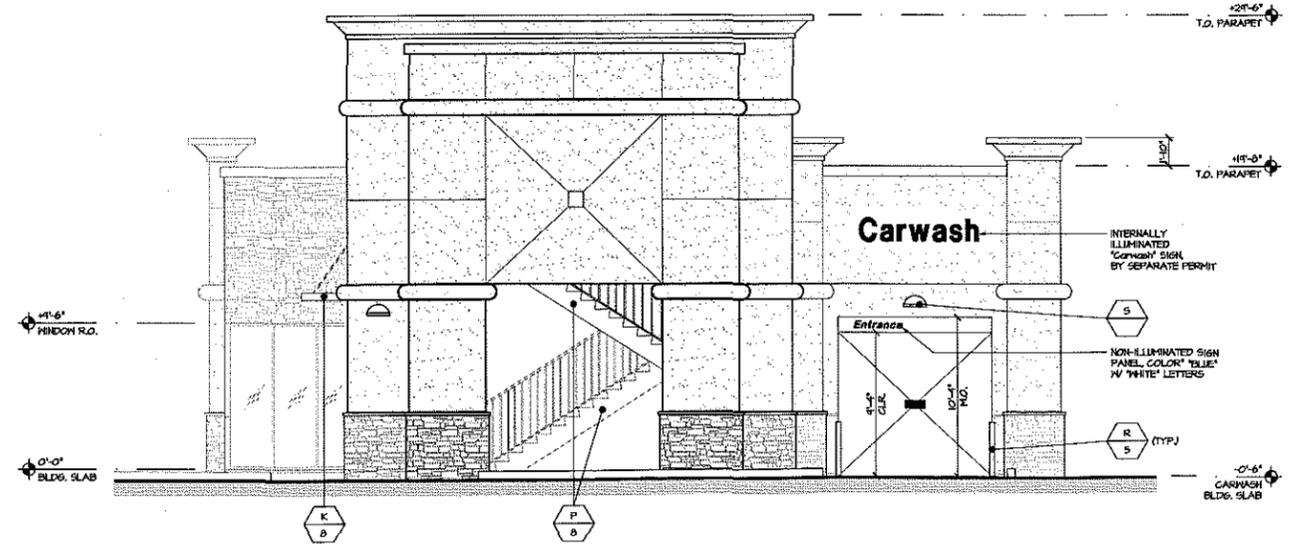
08/07/2004 12:15 PM 3 CIVIL ENGINEER Stevan Nakashima, CONSULT



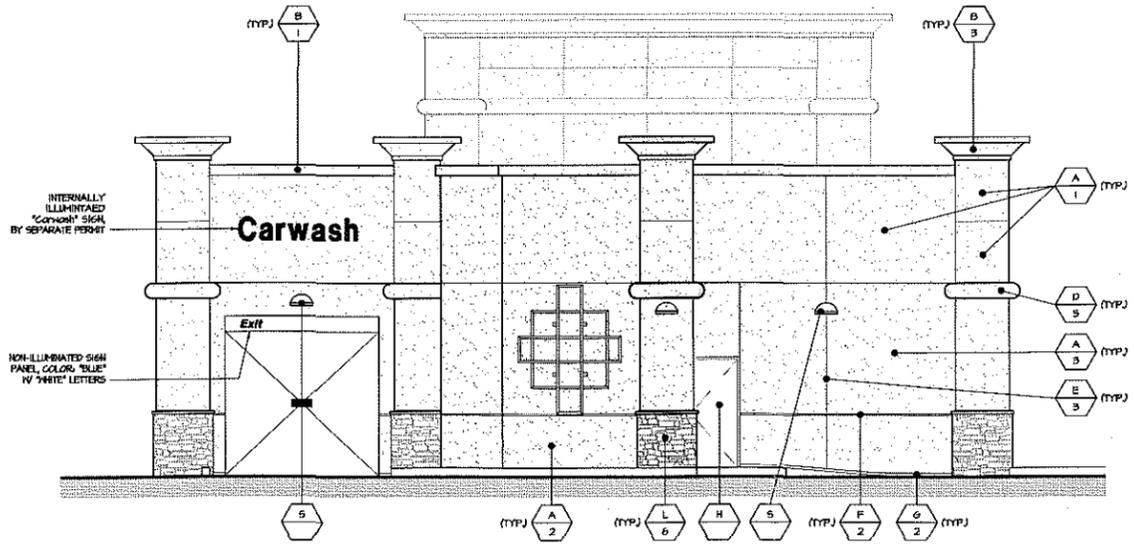
A NORTHEAST BUILDING ELEVATIONS
SCALE: 3/16" = 1'-0"



B SOUTHWEST BUILDING ELEVATIONS
SCALE: 3/16" = 1'-0"



C NORTHWEST BUILDING ELEVATIONS
SCALE: 3/16" = 1'-0"



D SOUTHWEST BUILDING ELEVATIONS
SCALE: 3/16" = 1'-0"

ELEVATION KEY NOTES

MATERIALS	
(A)	SMOOTH FINISH CEMENT PLASTER FINISH (PAINTED)
(B)	SMOOTH FINISH CEMENT PLASTER CORNICE WITH GALVANIZED STEEL (PAINTED)
(C)	DARK BRONZE ANODIZED STOREFRONT & DOORS W/ INSULATED GLAZING
(D)	EPS. FOAM SHAPE (PAINTED)
(E)	CEMENT PLASTER CONTROL JOINT (PAINTED TO MATCH ADJACENT WALL)
(F)	1" CEMENT PLASTER REVEAL (PAINTED TO MATCH ADJACENT WALL)
(G)	KEEP SCRIBED (PAINTED)
(H)	METAL DOOR & FRAME (PAINTED TO MATCH ADJACENT WALL)
(J)	METAL ANNING (PAINTED)
(K)	METAL TRILLES (PAINTED)
(L)	STONE VENEER
(M)	NOT USED
(N)	12" x 12" TILE ACCENT
(P)	EXTERIOR METAL STAIR (PAINTED)
(R)	GUARD POST (PAINTED)
(S)	EXTERIOR WALL SCORCE W/ BRONZE FINISH

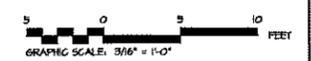
FINISHES	
(1)	PAINTED PPS #ICS-101-PPS 'CAPE AL LANT'
(2)	PAINTED PPS #ICS-104-PPS 'MOCHA'
(3)	PAINTED PPS #ICS-102-PPS 'NIMES'
(4)	PAINTED PPS #430-1 'GILLED WINE'
(5)	PAINTED #44-035-3 'WHITE'
(6)	EL. DORADO STONE, WESTERN PROFILE, HEATHER EDGE, COLOR, 'WESTERN WISCONSIN'
(7)	TO MATCH 'GILLED WINE'
(8)	PAINTED K1 #403H 'FINE FOREST GREEN'



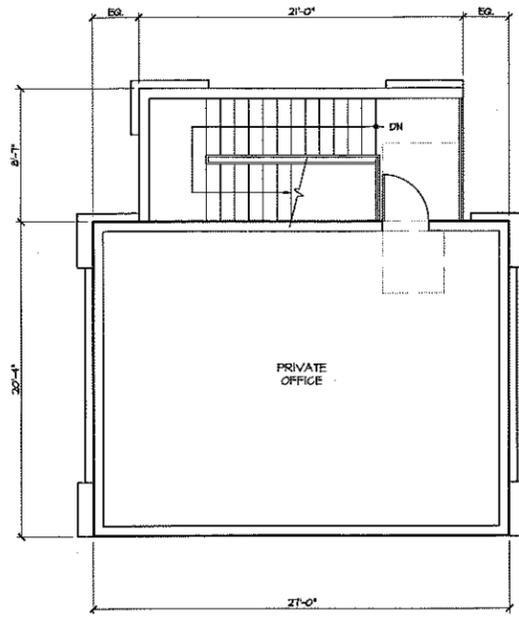
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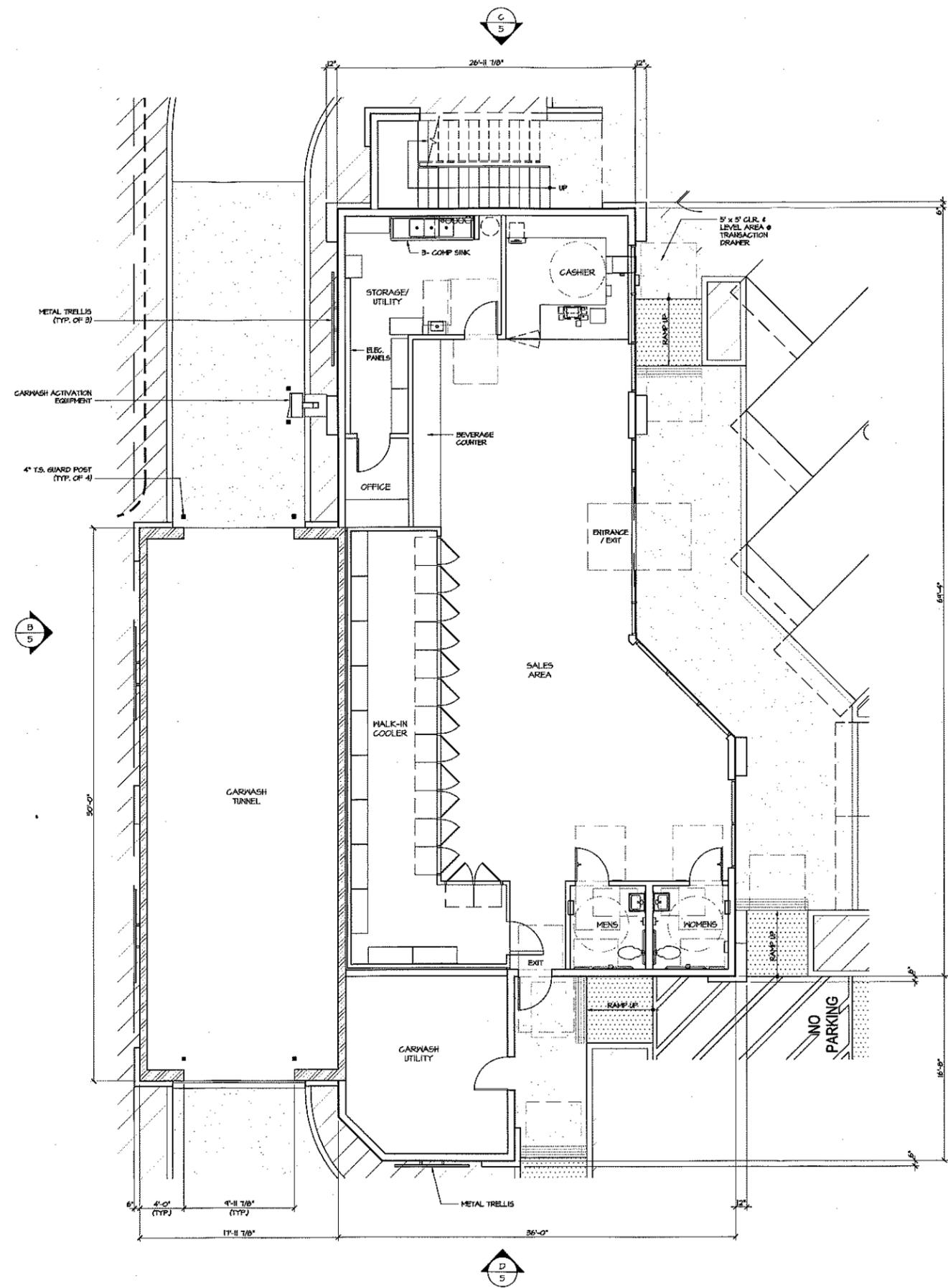
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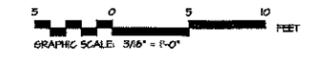
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2 2ND FLOOR PLAN
SCALE: 3/16" = 1'-0"



1 1ST FLOOR PLAN
SCALE: 3/16" = 1'-0"



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-	ISSUED FOR PLAN CHECK	
08-08-01	ISSUED FOR PLANNING	
NO.	DATE	DESCRIPTION
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CONCEPTUAL FLOOR PLAN		
PROJECT #: 08-5001		
DRAWN: JM CHECKED: MI		
SCALE: AS NOTED DATE: 08-08-08		
GENERAL DEVELOPMENT PLAN EXHIBIT C		

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