

ATTACHMENT 1: CUMULATIVE AMENDMENTS WITH CHANGES IDENTIFIED

Jackson-Taylor Residential Strategy

*Originally Approved by the City Council on October 6, 1992
and*

Amended November 16, 1996, ~~November 20, 1997, and May , 2008~~ [S]

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Prepared for:

**Department of ~~Planning, Building and Code Enforcement~~ [S]
City of ~~San José~~**

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**Prepared by:
Calthorpe Associates
and
Bay Area Economics**

Executive Summary

The "Jackson-Taylor Residential Strategy" is an outgrowth of the "Jackson-Taylor Neighborhood Revitalization Plan," adopted by the City in 1987. One recommendation of the Revitalization Plan was to convert an older industrial area located in the heart of one of San José's residential areas to uses that complement the adjacent neighborhood and provide additional market support for the nearby business districts.

The roughly 75 acre area is located near downtown retail and commercial uses, the San José Civic Center; North First Street commercial corridor, and Norman Y. Mineta [S] San José International Airport, which provide substantial employment opportunities and important services to the neighborhood's residents. Likewise, the Japantown [S] Neighborhood Business District directly west of the study area along Jackson and Taylor Streets provides local-serving goods and services, as well as restaurants and entertainment uses that draw patrons from throughout the region.

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The Jackson-Taylor Residential Strategy recognizes that the existing industrial uses in the area are already undergoing transition and that a land use pattern that maintains jobs, yet is oriented towards providing housing will be more compatible with the surrounding residential neighborhood. To this end, the plan recommends creating a mixed-use residential and commercial core in the center of the study area that is urban in character and at a scale and density that makes change economically feasible. The surrounding single-family neighborhoods would be buffered from the higher density development by moderate density housing whose design would respect and echo many qualities of the older neighborhoods. The illustrative land use plan suggests locations for specific uses with the study area. Ground floor retail space in selected locations would provide neighborhood-serving and specialty shopping in a manner that complements the Japantown, [S] Business District, yet supports employee and resident needs. Public amenities, such as day care and a community center would be located in an expanded Bernal Park. Streets would be opened to re-establish the block pattern of the surrounding area and provide comfortable and pleasant routes for pedestrians. Small parks, connected by these pedestrian streets, would be sited throughout the area to provide relief from the urban environment and to allow public outdoor activities. These elements seek to implement many of the plan's objectives to maintain and enhance the character of the surrounding neighborhood; provide a range of housing types and densities; encourage a mix of housing, employment, shopping and public uses; enhance public open space opportunities; and strengthen pedestrian and street connections within the neighborhood.

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The design, configuration and mix of uses is also intended to create a pedestrian-oriented neighborhood that supports the use of public transportation. The plan mixes residential, retail, office, community facilities, parks and nearby schools within a comfortable walking distance, making it convenient for residents and employees to travel by transit, bicycle or foot, as well as by car. Bus stops within the study area would continue to provide fast and efficient service to the Guadalupe Light Rail Transit Corridor and to other downtown destinations. Connections to the Guadalupe Light Rail Transit Corridor also provide opportunities for travel to all stations on the VTA Light Rail network, including North San José. [S]

Deleted: Additionally, BART is considering extending service to San Jose underground along the railroad alignment that passes through the study area. A BART stop is designed into the plan and can be considered a very valuable amenity that would complement the planned mix of uses.

Policies, design guidelines, and implementation strategies tailored to the conditions of the Jackson-Taylor area, are also provided to assist the City and property owners in designing and building new projects and public improvements. They aim to implement the most important urban design principles of the illustrative land use plan, yet be flexible enough to provide opportunities for new, innovative site plans and building designs.

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The Jackson-Taylor Residential Strategy is consistent with the San José 2020 General Plan [S]. The General Plan establishes the land use pattern, development intensities and policy considerations for this area by creating the Jackson-Taylor Planned Residential Community (PRC). The PRC incorporates the major features of the illustrative land use plan and policy direction contained in the Residential Strategy. The Jackson-Taylor Residential Strategy is a separate policy document, providing background, vision, and community character for the PRC and also a level of detail for implementation beyond the scope of the General Plan.

1. Introduction

In 1987, the San José City Council adopted the “Jackson-Taylor Neighborhood Revitalization Plan” for an approximately 80-block area located just north of Downtown, bounded by the Guadalupe Parkway, East Julian, North Thirteenth, and East Hedding Streets. This plan developed a set of long-range guidelines and implementation strategies for the preservation of existing housing stock, enhancement of neighborhood business districts, and conversion of industrial lands to residential and commercial uses. The plan recommendations were based on input and suggestions from an extensive two-year long public participation process and consultant assistance for economic, land use, and urban design issues. Since adoption of the Revitalization Plan, the Council has successfully implemented many of the report’s recommendations.

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The “Jackson-Taylor Residential Strategy” is an outgrowth of this initial planning process. One recommendation of the Revitalization Plan was to convert an older industrial area located in the heart of the Jackson-Taylor neighborhood to residential and supporting commercial uses. The plan suggested that the area’s warehousing, food distribution, and manufacturing uses gradually transition to residential uses that complement the adjacent neighborhoods and provide additional market support for the nearby business districts. The “San José Housing Initiative Study” also identifies the industrial area as a suitable site for transit-oriented high density infill housing. The Jackson-Taylor Residential Strategy evaluates current land use and property utilization patterns in this industrial area and provides a series of policies and implementation recommendations for transition to residential and supporting commercial uses.

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2. Location and Context

The Jackson-Taylor Residential Strategy study area consists of approximately 15 blocks between East Hedding and East Empire Streets from roughly North 6th to North 11th Streets (see Figure 1). The roughly 75-acre area is located in the heart of one of San José's older residential areas, directly north of the Downtown Core Area. The nearby downtown retail and commercial uses, North San José, and the Norman Y. Mineta San José International Airport provide substantial employment opportunities and important services to the neighborhood's residents [S]. Likewise, the Japantown, [S] Neighborhood Business District directly west of the site along Jackson and Taylor Streets, provide local-serving goods and services, as well as restaurants and entertainment uses that draw patrons from throughout the region. Japantown is also an important focal point for Santa Clara County's Japanese community.

The site is well served by local streets, the regional freeway system, and the City's light rail and feeder bus transit system. North First Street, approximately 1/3 mile from the site, is an arterial with direct access to downtown, as well as employment centers to the north. The light rail line follows North First and stops at Ayer and East Mission Streets. Feeder bus lines run along East Hedding, East Taylor, and Jackson Streets to bus stops in the study area. A major spur of the Southern Pacific Railroad (SPRR) bisects the site from its northeast to southwest corner.

The surrounding Jackson-Taylor neighborhood is an older residential area with approximately 4,600 residents from diverse ethnic backgrounds and income levels. It supports strong Hispanic and Japanese communities, as well as a growing number of Filipinos. According to the "Jackson-Taylor Neighborhood Revitalization Plan" prepared in 1987, the larger Jackson-Taylor neighborhood's percentage of Spanish origin population is 46.2%, over twice the percentage for the city as a whole (22.3%). Japanese-American residents account for 5.8% of the area's population, compared to 1.9% of the City's population. Home ownership is limited to 30% of the neighborhood's households, about one-half the percentage for the city as a whole. Family incomes are also far below the city and county averages (1980 Census). Housing types include small single-family homes, duplexes, Victorians, two-story apartments, and a mid-rise elderly housing apartment building. While housing in the larger area is generally in good condition, there are some blocks where there is evidence of maintenance neglect. Most residential lots are 6,000 s.f. (7 u/ac). The predominate housing style is the California bungalow, with the exception of Hensley Historic District which represents the City's largest concentration of Victorian houses. Across East Hedding Street, along the northern edge of the study area, the land uses are primarily industrial in nature. In addition, the first fifteen years of implementation of the Jackson-Taylor Residential Strategy have resulted in high density housing located generally in the eastern portion of the study area. [S]

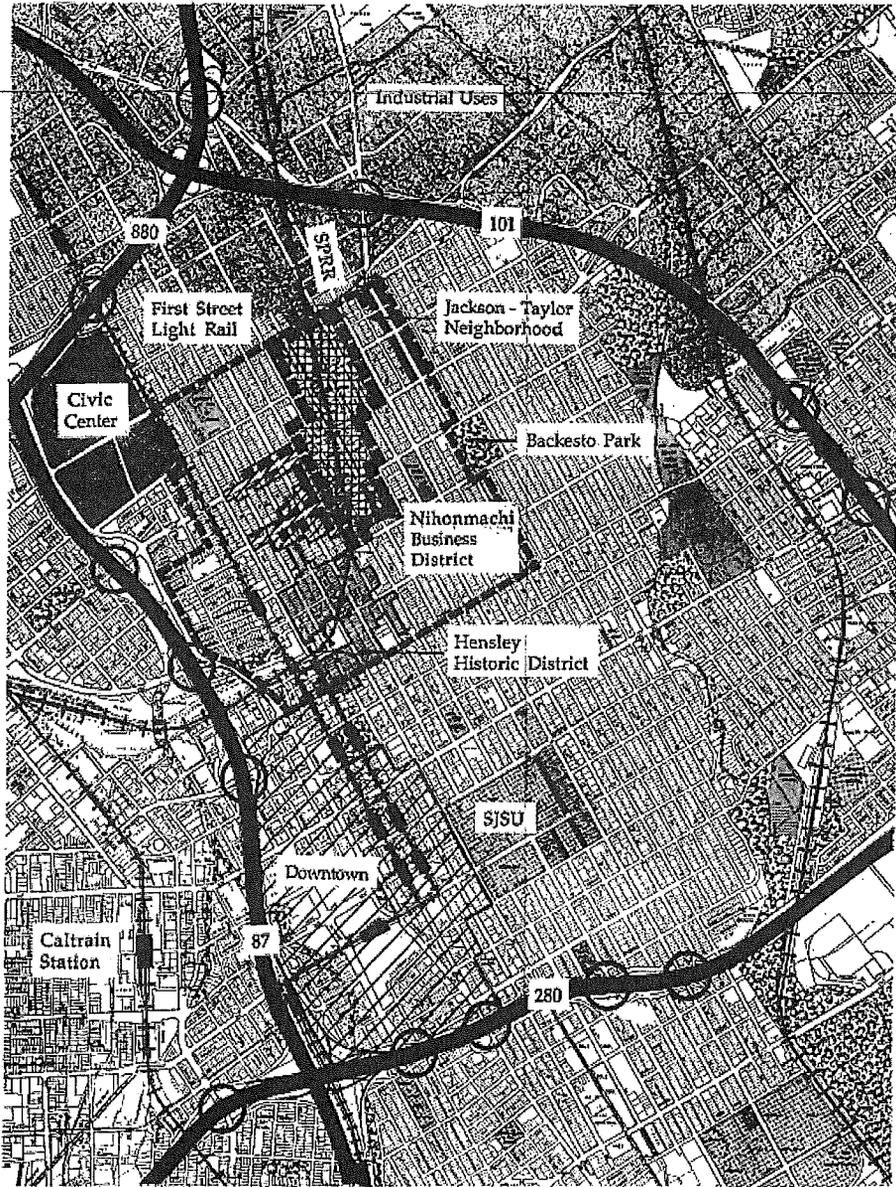
Two schools and parks are located within walking distance of the study area. Bernal Park, a 2.4-acre park with a tot lot and ball field, is located in the northwest corner of the study area. It is a heavily used park serving residential areas both north and south of East Hedding. Backesto [S] Park (10 acres) is located between Jackson and East Empire Streets five blocks east of the site. Grant Elementary School is two blocks east of the site, also between Jackson and East Empire Streets. Burnett Middle School is located four blocks west of the site on North Fourth Street

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between East Hedding and East Mission Streets [S]. Residents of the Jackson-Taylor neighborhood have indicated that the area has fewer parks per population than many other San José neighborhoods.

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AREA MAP

CALTHORPE ASSOCIATES
SAN FRANCISCO, CALIFORNIA
RAY AREA ECONOMICS
BERKELEY, CALIFORNIA

JACKSON-TAYLOR RESIDENTIAL STRATEGY
SAN JOSE, CALIFORNIA

FIGURE 1



3. History of the Site

The Jackson-Taylor industrial area has historically had strong ties to the food industry. Originally developed by Del Monte, Glorietta Foods, and Continental Can Co. as a series of fruit and vegetable canneries, the area grew around the Southern Pacific Railroad to serve the San José region's extensive orchards and farms. Remnants of this past are still present, such as Del Monte's trademark brick buildings and water tower (along 8th Street north and south of Jackson Street), the Continental Can building (on the northwest corner of 9th and Taylor Street), and Glorietta's overhead conveyor belts. These landmarks establish important elements of the area's character. The southern brick Del Monte building (561 N. 9th Street) is listed on the City's Historic Resources Inventory.

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Once the region's orchards and farms moved to the San Joaquin and Central Valleys as a result of rapid urbanization, the canneries closed and were eventually sold. While some food processing uses still remain today, the majority of the canneries transitioned to food-related packaging, warehousing, and distribution facilities. These businesses bring large shipments from farms or food product manufacturers, break them down into smaller increments, and distribute these products to grocery stores, restaurants, and catering businesses throughout San José.

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Most recently, the Jackson-Taylor industrial area is experiencing another transition. The area's low rents and easily subdivided older warehouse buildings have attracted a number of small incubator manufacturing and auto-detailing businesses, as well as rehabilitated office space for professionals, non-profits, artists, and performance groups.

4. Existing Conditions

The following is a summary of the key findings of the staff/consultant Existing Conditions/Opportunities and Constraints Analysis. The analysis assesses the physical and policy factors affecting transition from industrial to residential uses.

A. Land Use

Land uses in the Jackson-Taylor industrial area are a mix of food-related and other warehousing and distribution facilities, light industrial manufacturers, auto parts and detailing uses, small office space, restaurants, outdoor storage, vacant land, and residences. The City of San José owns two large parcels in the area, Bernal Park and the City's former Corporation Yard site [S]. The City relocated the Corporation Yard in 2007, and the Redevelopment Agency commenced demolition of the buildings on the Corporation Yard in January 2008. [A] Figure 2 illustrates existing land use patterns.

Close to 10 acres within the study area are either vacant, contain abandoned buildings, or are devoted to open storage or truck parking. These parcels are the most readily available for conversion to other uses because they require little business relocation and building demolition. Key vacant parcels include [S] the block bounded by East Taylor, North 8th, Jackson, North 7th Streets, and bisected by the railroad tracks; the parcel at the southwest corner of East Hedding and North 10th Streets; and the former Corporation Yard. [S]

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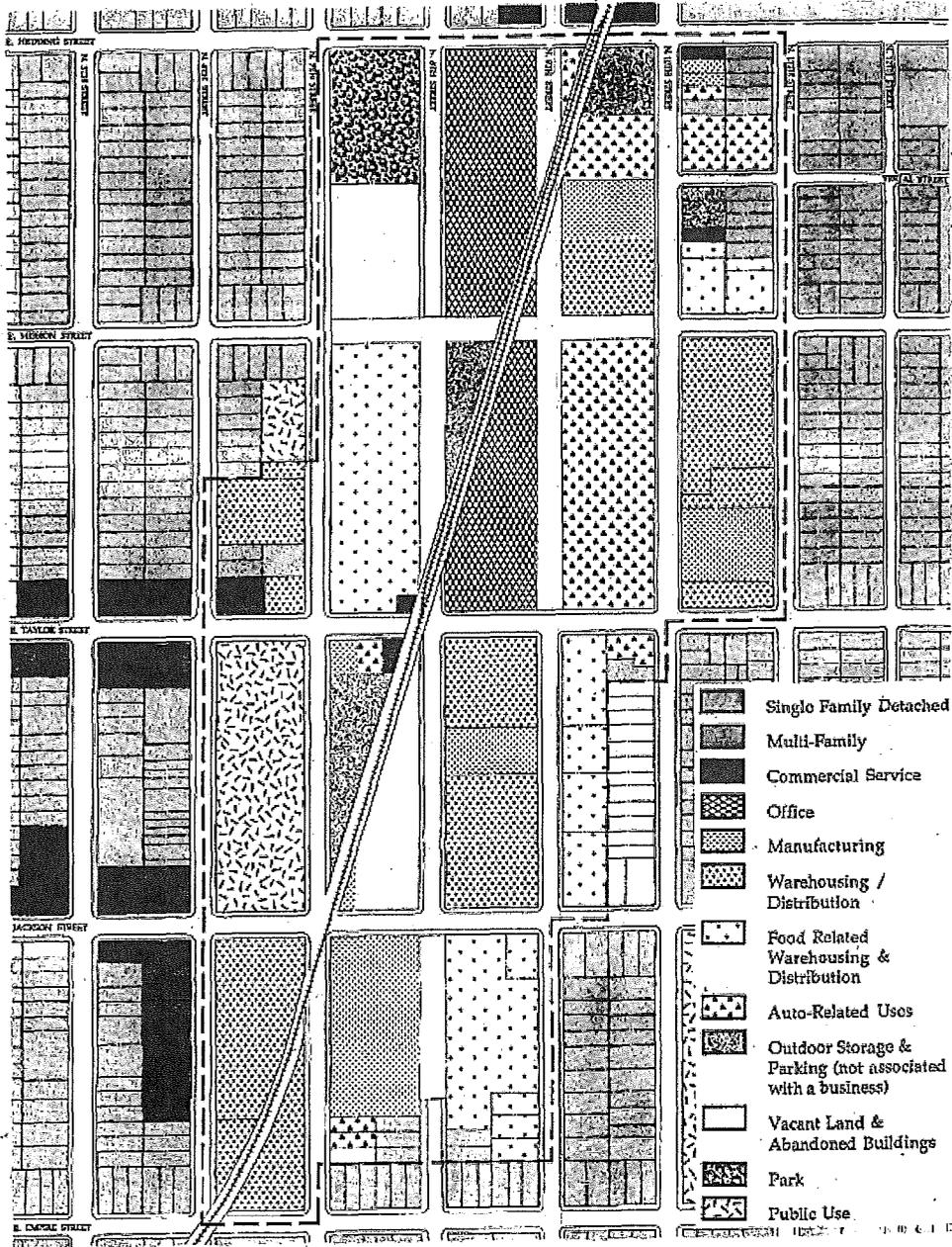
B. Ownership Patterns

Property ownership in the study area is dominated by a single interest, and influenced by several moderate-sized owners (Figure 3). Metropolis West controls over 24 acres in the study area, and is actively seeking options on several other properties. Metropolis West redeveloped the Cannery Park I and II complex, located between East Taylor, North 10th and the railroad, and is in the process of renovating the buildings on the northwest corner of Jackson and North 9th Streets. They also own the triangular-shaped block bounded by Jackson, North 8th, and the railroad tracks where an abandoned Del Monte brick building is located, as well as the block directly east of Bernal Park, the parcel south of Bernal Park, and the small triangular-shaped parcel at the southeast corner of East Mission and North 8th Streets.

Other major land owners include the City of San José which owns the Corporation Yard and Bernal Park, the San José Produce Terminal, the former Mariani Packing Partnership, the former XDM Properties site [S], and the Milton Fox Company. Small parcels, with separate owners, are generally located along the northeastern and the southern boundaries of the study area where single family homes are located.

C. Site Conditions

The site is located in the flat valley plain of old central San José. The area's finely gridded street system parallels the eastern foothills and is an extension of the block pattern which originates downtown, extends through the site, and terminates to the north at the Interstate 880/Highway

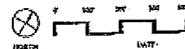


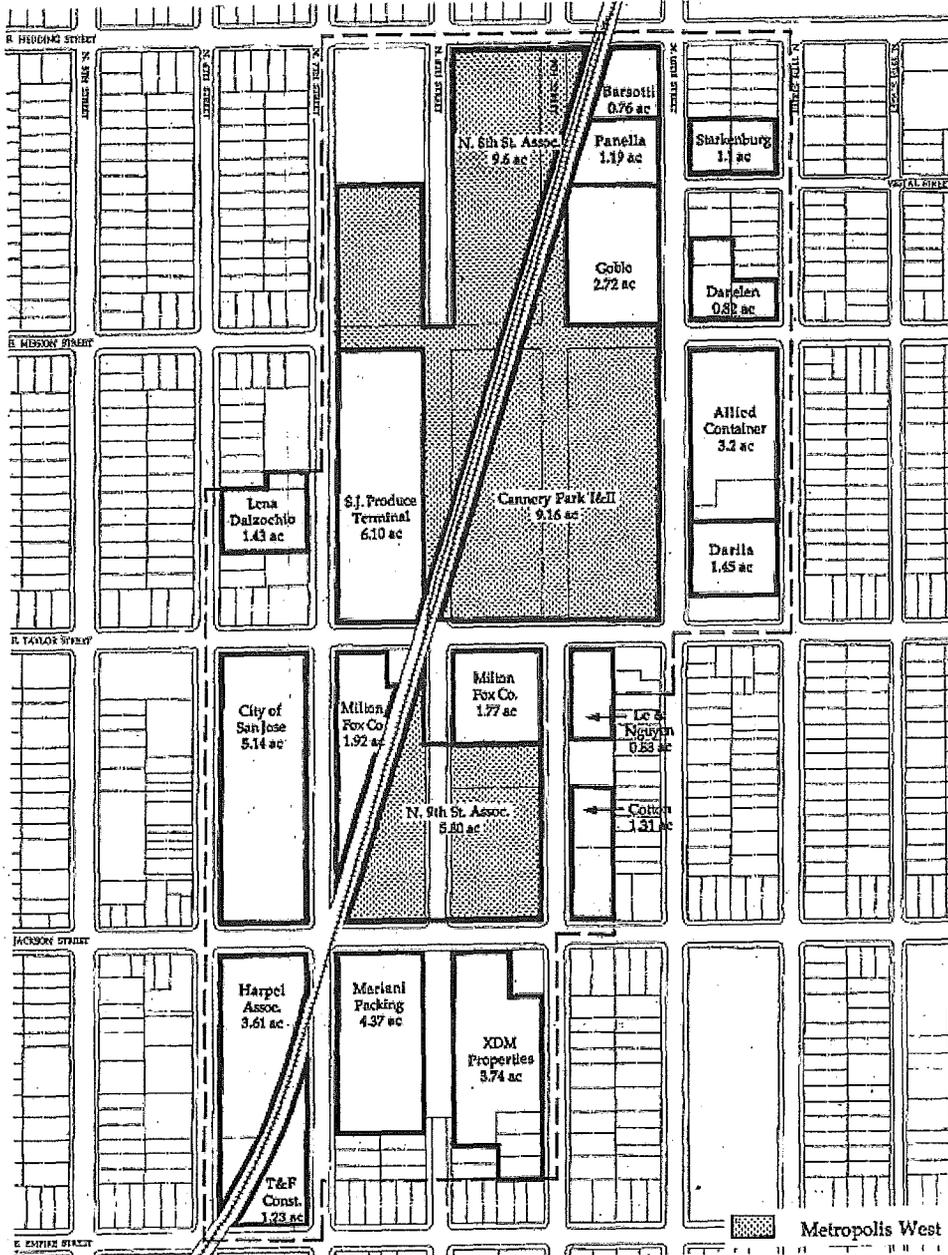
EXISTING LAND USE

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 SAN FRANCISCO, CALIFORNIA
 BAY AREA ECONOMICS
 BERKELEY, CALIFORNIA

JACKSON - TAYLOR RESIDENTIAL STRATEGY
 SAN JOSE, CALIFORNIA

FIGURE 2





PROPERTY OWNERSHIP
 (excluding single family)
 JACKSON - TAYLOR RESIDENTIAL STRATEGY
 SAN JOSE, CALIFORNIA

CALTHORPE ASSOCIATES
 SAN FRANCISCO, CALIFORNIA
 BAY AREA ECONOMICS
 BERKELEY, CALIFORNIA

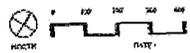


FIGURE 3

01 interchange. Open corridors created by the streets provide views to the eastern hills, as well as occasional glimpses to tall downtown buildings. Block sizes are typically 300 by 850 feet. Streets typically have 80 foot rights-of-way; not all are improved with curbs and sidewalks. In the surrounding residential area, some blocks are also served by narrow alleys.

The overall character of the area's streetscape is barren, and in marked contrast to the extensive tree canopy of the adjacent residential areas. Sidewalks are not continuous throughout the area, and with few exceptions, street trees are not planted. In keeping with the current industrial nature of the area, parking lots and loading docks are often fenced with chain link and barbed wire. This character abruptly changes at the border of the Japantown [S] Business District, which has recently made extensive streetscape improvements to encourage pedestrian activity.

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Several streets in the study area have been closed or abandoned by the City. Most of these closed streets are being used for additional parking or circulation space. Buildings are not allowed on these closed rights-of-way [S]. These streets include East Mission between North 7th and 10th, North 9th between East Hedding and East Taylor, and North 8th from Jackson Street south for 3/4 of a block.

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D. Proposed Projects

Japantown [S] Business District

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The "Jackson-Taylor Neighborhood Revitalization Plan" identifies the Japantown [S] Business District as an area appropriate for revitalization to serve the shopping and cultural needs of both the surrounding residential neighborhood and the larger San José Japanese community. In 1987, the City Council approved and accepted the "Nihonmachi Business District Plan" which addresses long-range land uses, public improvements, parking, building design, and other infrastructure improvements along the north and south frontages of Jackson Street between North 3rd and North 6th Streets. Since adoption, additional street trees have been planted, 6th Street has been restriped [S] to allow diagonal on-street parking, historic banners and plaques have been placed along Jackson Street, and the Ayer light rail station has been renamed to the Japantown/Ayer Station; utilities are currently being placed underground. The Business District has since been enlarged and plans for streetscape improvements have been extended to include Taylor Street.

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The Business District Plan also calls for additional street furniture, acquisition of off-street parking lots on one or more sites, diversification of retail and service uses along Jackson Street, construction of a community center, and redevelopment of the City's Corporation Yard.

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City of San José Corporation Yard

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The Corporation Yard, located on the block bounded by North 6th, North 7th Jackson, and Taylor Streets, served [S] as the City's main service yard for the General Services Administration, Building Maintenance, Vehicle Maintenance, Warehousing, Materials Lab, Parking Meter Repair, and Street Sweeping functions of the city. The 5.2 acre site was [S] developed with a series of building, mechanical equipment, and parking areas.

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In 2007, the City of San José expanded operations at its Central Service Yard on Senter Road and the uses of the Japantown Corporation Yard were transferred to the Central Service Yard. The Redevelopment Agency commenced demolition activities at the site in 2008. [S]

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In 1987, the City Council began considering land use changes on the Corporation Yard site to implement the recommendations of the "Jackson-Taylor Neighborhood Plan" and the "Neighborhood Business District Plan" to redevelop the site into uses that are compatible with the Neighborhood Business District. In 2007, the City considered a proposal to include up to 600 market-rate residential units, 16,000 to 30,000 square feet of retail space, a 10,000 to 20,000 square foot community amenity space, and up to 934 underground/surface parking spaces. As a variation on the proposal, up to 14,000 square feet of retail space would be replaced with up to 24 live/work units. Project buildings would range from 6 to 14 stories for the residential/mixed uses and 1 to 2 stories for the community amenity uses. [A]

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Metropolis West

As mentioned above, Metropolis West owns or has options on a significant portion of the study area. Their most recent project is Cannery Park I and II, a 9.16 acre renovation of older industrial buildings to small office and light industrial space. Building design and improvements are targeted to suit the needs of new "incubator" businesses that require inexpensive space. Cannery Park is not fully completed. Metropolis West plans to make additional improvements to the facilities to serve other similar uses, including arts and non-profit groups. All of the improvements made to date are considered by the developers as "interim improvements;" in the long term, Metropolis West plans to build higher intensity uses on their property. No specific plans have been discussed for the group's other properties in the study area, although the developer has indicated an interest in a coordinated mixed-use development.

Deleted: Redevelopment would entail relocation of the Corporation Yard to an alternative site in the city. In 1988, the San Jose Nihonmachi Corporation submitted a preliminary proposal to the Council to develop a "Japanese Village" on the Corporation Yard site and an additional parcel on 6th Street. This mixed-use project, as proposed, would consist of a hotel, senior housing, cultural museum, retail space, and underground parking. Uses would be located in two to six story buildings fronting onto a central mid-block mall. The City Council has indicated interest in the proposal and has set aside funds in its 1990 budget to locate an alternative site for the Corporation Yard.

E. Plans and Policies

The San José 2020 General Plan identifies the study area as within the Jackson-Taylor Planned Residential Community (PRC), which was created to increase high density housing opportunities and supportive mixed uses in the central area of the City and in close proximity to transit. Light rail transit, public bus services, regional freeways, and local streets provide transportation access, connecting the PRC to San José's job centers in Downtown, North San José, and Edenvale. The PRC is adjacent to the Japantown Neighborhood Business District which offers commercial services, restaurants, and specialty stores which serve both the immediate neighborhood and the region. Land use designations within the PRC allow for residential, mixed use, general commercial, light industrial, and public park/open space uses. [S]

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The perimeter of the study area is designated for a variety of land uses including High Density Residential uses around Bernal Park (12-25 du/ac); Mixed Use [S] with a Neighborhood Business District Overlay on the City's Corporation Yard and the Harpel property along Jackson Street; and Medium Density Residential (8 du/ac) where existing residential structures are located. A Mixed Use Overlay is shown on four parcels north of Jackson on either side of North 9th Street. This overlay allows for developments consisting of a combination of residential, commercial, and industrial uses. The southern portion of the study area, below Taylor Street, is

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within the Downtown Frame Area. Uses within the Downtown Frame are allowed to develop at higher densities and with building heights of up to 120 feet (as opposed to the standard 50 [S] foot height limit).

The study area is mostly zoned for either Light Industrial uses (LI) or Planned Development (PD). [S]

F. Circulation and Transit

The street system in the study area is a fairly continuous extension of the city's old gridded street pattern. East Hedding Street is a four-lane east-west arterial which originates at the Civic Center on the west. North 10th and North 11th Streets form a one-way north-south couplet with three lanes in either direction.

Traffic, which is primarily through traffic, is very heavy along East Hedding, North 10th and North 11th Streets, and at moderate levels along Jackson and Taylor Streets where the commercial uses are located. The intersection of North 10th Street and East Hedding is constrained, particularly given recent construction of the Cannery Park project. Trucks frequently use the study area streets to access industrial uses and will continue as long as these uses remain active. Local residents and business owners have repeatedly indicated that on-street parking is limited and in short supply.

The area is very well served by transit. The North First Street Light Rail Line is within 2,500 to 4,000 feet of the site, which is a reasonable walking distance. Additionally, feeder bus lines run east-west to and from the light rail line and north-south to Downtown. [S]

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5. Potential for Change

A. Issues and Analysis

Over the last quarter century many industrial sectors within the U.S. economy have undergone radical restructuring that has, in turn, affected the land use patterns of most cities. In San José, this restructuring had its most pronounced impact on the food processing and food packing industries that were prevalent throughout Santa Clara County until after World War II. The Jackson-Taylor industrial area was one of the last victims of this restructuring in that the area's major land use, Glorietta Foods, did not cease operations until the early 1980's. However, once Glorietta closed its operation, a vacuum was created. This old industrial space was either too big or too antiquated to serve the needs of other large-scale manufacturing operations. In addition, most of the newer manufacturing activity in San José at this time was oriented towards high technologies sectors and was looking to locate in more recently developed areas with better facilities.

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In 1985, the San José City Council commissioned a study of the Jackson-Taylor neighborhood, including the industrial district. One focus of the plan was to determine what new land uses could fill the vacuum created by the closing of food processing operations in the area and how these uses could be compatible with the surrounding residential area. New uses could take advantage of the area's excellent location relative to both downtown San José, which was itself experiencing a renaissance, as well as excellent access to the city's major freeway network, airport, and new office parks growing up around the airport. The result of this study was the "Jackson-Taylor Neighborhood Revitalization Plan," which was adopted in 1987. This report recommended that the industrial area be redeveloped with a mix of residential and supporting commercial uses.

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In the intervening period between the time that the 1987 study was completed and a new [S] study was begun in 1990, much of the vacant space in the Jackson-Taylor study area had been subdivided into smaller spaces suitable for a variety of small-scale manufacturing, warehousing, automotive related, and other miscellaneous activities. While some of this activity could clearly be considered as interim land uses that are occupying space until it is redeveloped, the area could also be viewed as a viable small-scale light manufacturing/heavy commercial district that could continue to function as such indefinitely. This demonstrates that the industrial real estate market within San José is strong enough to essentially revitalize or, at least, reuse these older industrial facilities for new uses that are more appropriate to the City's current economic conditions.

This "revitalization process" in Jackson-Taylor has also been facilitated by the City's own public policy by virtue of the fact that the area is part of San José's Enterprise Zone. All businesses located in this Zone are entitled to certain tax benefits as well as having access to other special services. These benefits have been an effective mechanism for attracting new businesses to the area. For example, Mariani Packing Company, which employed over 200 people, came to the area in the early 1980's to take advantage of these benefits [S]. The incentives provided by the Enterprise Zone status may be a limitation to effectively converting this area to non-industrial residential and commercial uses.

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The current Jackson-Taylor Residential Strategy is intended to implement the mandate of the City Council by formulating a plan for the study area that encourages transition to a mix of residential and supportive mixed-uses. A critical element of the planning process has been to determine how susceptible to change the existing land uses in the area might be, and to determine how the plan could be phased so that new development will not push out existing uses unnecessarily before they are ready to leave.

In order to better understand the dynamics of the existing land uses in the study area, as well as to gauge how soon these uses might be likely to turn over, the economic viability of its current uses was evaluated. A variety of data sources were used to assess current economic conditions, including information of the assessed valuation of land and improvements for each parcel, interviews with the major property owners within the study area, a survey of all businesses within the study area, interviews with local real estate brokers familiar with the light industrial/heavy commercial markets in San José, and interviews with experts regarding the current status of the food processing industry in San José.

B. Economic Viability of Existing Uses and Phasing Issues

The following is a summary of the findings of the economic viability and phasing analysis. Site specific assessment of the potential for change is shown on Figure 4.

- The number of stable uses in the study area strongly indicates that transition to entirely residential uses will not occur in the near term. This will greatly affect the type of residential uses that will be interested in locating in the area and its potential character.
- A mix of industrial, residential, and commercial uses in the central portion of the study area will be much easier to achieve because this portion of the study area is separated from the surrounding single-family residential area and future land uses will have fewer impacts on this neighborhood.
- The pattern of stable uses and parcels that are likely to redevelop immediately is very scattered. Highly developable parcels are not clustered into one central area, rather, they are separated by several blocks. This will affect land development and circulation patterns in the near term. For example, development of the parcels around Bernal Park has resulted in weak pedestrian connections because of the railroad tracks and large industrial block pattern existing in the study area. [S]
- The lack of immediately developable parcels directly adjacent to the Neighborhood Business District on Jackson Street will make it more difficult to initially establish a strong connection between the shopping area and new uses in the study area.
- Stable industrial uses that do not redevelop in the near term will likely seek to establish buffers along their edges to avoid nuisance complaints from future adjacent neighbors.

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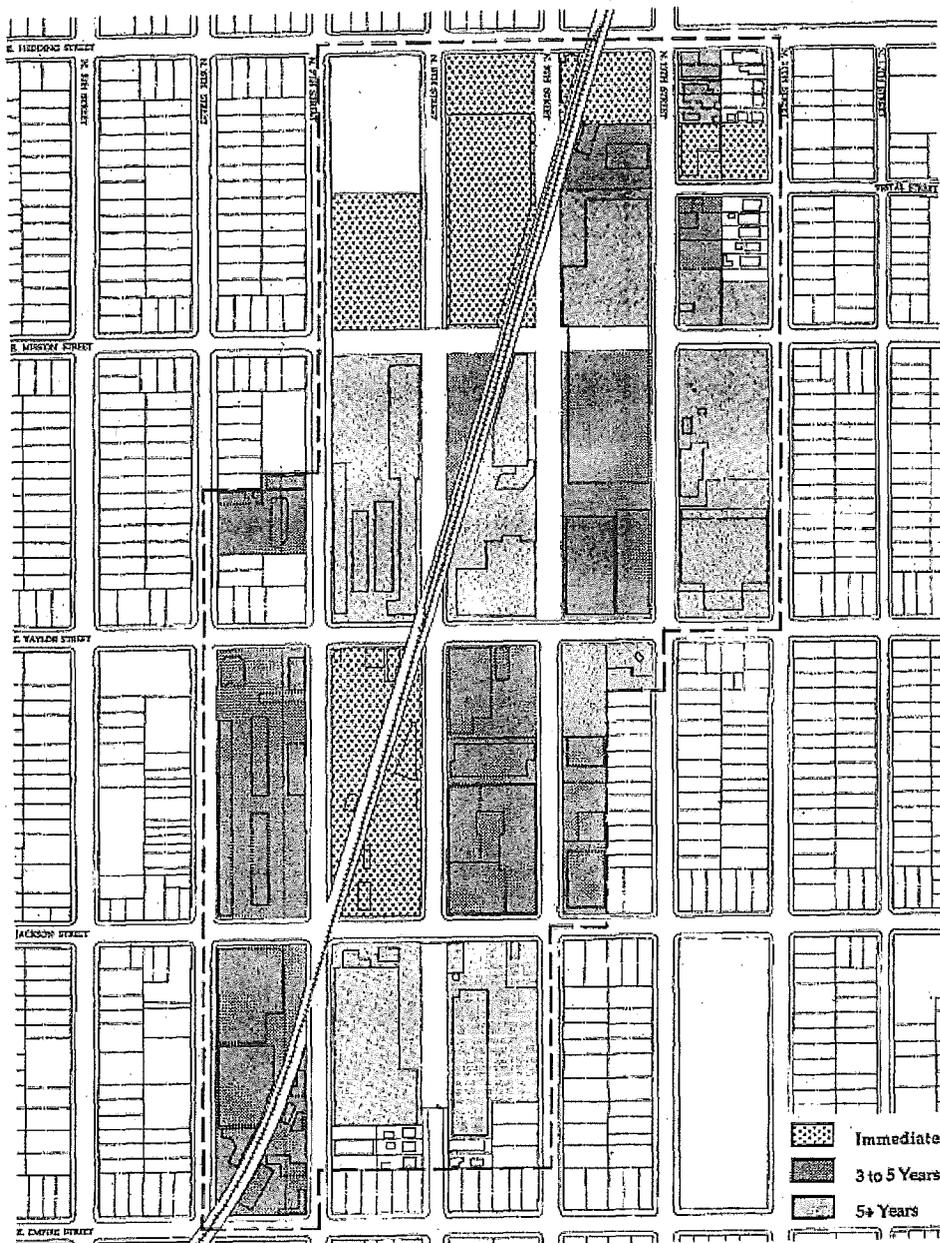
Deleted: are able to redevelop soon, residents would have to walk through or by the San Jose Produce Terminal, an active industrial area, to get to the shopping district. This will be a

Deleted: and somewhat of a barrier to resident shopping in the neighborhood.

C. Economic Analysis

The Jackson-Taylor study area was identified as a potential high-density [S] housing site in the Housing Initiative, a detailed study of high density housing opportunities along transit corridors in San José. As part of the Housing Initiative, economic consultants prepared detailed financial feasibility analyses of “prototypical” housing projects ranging from 18 to 70 units/acre. All of the analysis done as part of the Housing Initiative directly applies to the Jackson-Taylor area and the consultants have utilized this information in developing their recommendations for Jackson-Taylor. The Housing Initiative analysis concluded that development of for-sale housing between 18 and 70 du/ac was economically feasible, but that rental housing would generally require some form of subsidy.

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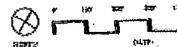


ECONOMIC POTENTIAL FOR LAND USE CHANGE

CALTHORPE ASSOCIATES
SAN FRANCISCO, CALIFORNIA
BAY AREA ECONOMICS
NAPERVILLE, ILLINOIS

JACKSON - TAYLOR RESIDENTIAL STRATEGY
SAN JOSE, CALIFORNIA

FIGURE 4



6. The Land Use Plan

The Jackson-Taylor Residential Strategy has evolved out of a complex balancing of community concerns, market economics, urban design principles, and technical feasibility. Each of these viewpoints has been strongly considered throughout preparation of this plan, and has helped to lay a foundation for its acceptance and implementation. The result is a program for transition from primarily industrial businesses to a mix of housing, commercial, and public uses within a vibrant, pedestrian and transit-oriented setting.

A. Objectives and Planning Process

The existing industrial area is located at the juncture of a number of cultural and political communities. Each group, whether they are homeowners, renters, business or property owners, brings a different issue or concern to bear in considering appropriate futures for the industrial area. During preparation of this plan, a Task Force was formed, drawing from these wide-ranging viewpoints. Their concerns for preserving jobs, providing affordable and owner-occupied family and senior housing, maintaining the economic vitality of Japantown, protecting the surrounding single-family neighborhood, supporting cultural values, and creating a plan that can realistically be implemented, have been the primary basis for planning decisions. Because the Jackson-Taylor Task Force represented a wide and sometimes conflicting range of interests in the area, members disagreed on a number of issues and majority votes did not always represent a complete consensus. The resulting plan is a compromise between a variety of viewpoints.

Two issues that Task Force members discussed at great length throughout the process were affordable housing¹ and gentrification. Although unable to quantify the potential for gentrification, some members of the Task Force were concerned that residents of the surrounding area could be displaced as a result of new development in the Residential Strategy study area. To mitigate gentrification effects, some members suggested that the Residential Strategy incorporate City programs that might mitigate displacement and improve the area's owner/renter ratio (such as first time homebuyer loans, down payment assistance, home improvement loans, etc.) and require some minimum percentage of affordable housing for new residential development in the study area. Other Task Force members thought it was inappropriate for the Residential Strategy to include policies directed at areas outside the study area boundary. Some members were also concerned that significant amounts of subsidized, low income rental housing in the study area could deter the production of market rate housing.

Despite disagreement on these issues, the Task Force established the following objectives for the plan:

¹ Affordable housing is defined as housing that is available at affordable cost to very low, low and moderate income households. Very low income households are defined as those households who earn no more than 50% of median income, adjusted for family size. While nationally, low income households are generically defined as households with incomes not exceeding 80% of median, low income for San José in 1990 has been adjusted by the U.S. Department of Housing and Urban Development and the State to refer to those households with incomes not exceeding 66% of median (due to San José's high median household income levels). Moderate income households are those earning no more than 120% of median income. Appendix A shows the 1990 maximum income levels for very low, low and median income households for San José.

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Land Use

- Provide for a mix of housing types, densities, and prices
- Encourage supportive mixed uses, including, but not limited to:
 - Job-supporting uses
 - Day care centers
 - Churches
 - Cultural/community centers
- Improve the quantity/quality of neighborhood open space
- Minimize alcohol-related uses
- Discourage new homeless shelters in the study area

Neighborhood Character

- Maintain existing neighborhood character
 - Encourage development which enhances the area and is compatible with existing land uses
 - Shape new development at the edge of the study area to reflect the character and rhythm, if not the exact scale, of existing surrounding residential development
- Strengthen pedestrian linkages to adjacent neighborhoods, transit, and Japantown, [S] Neighborhood Business District
- Maintain existing street pattern

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Using these objectives, the planning team prepared three alternative plans for review and evaluation by the Task Force:

- 1) Low Intensity/Minimum Change. Encouraged re-use of existing buildings whenever possible and low density industrial, commercial, and office uses.
- 2) Mixed-Use Core/Residential Edge. Proposed a mix of office, residential and retail uses in the center of the site, with moderate and high density housing adjacent to the existing neighborhood.
- 3) Urban Residential. Maximize [S] the amount of housing on the site with minor supporting retail uses.

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The Task Force identified the second alternative as best meeting their objectives. This alternative was then refined and developed into a preferred land use plan.

The Task Force process resulted in the approval of a plan that could accommodate approximately 2,150 housing units, 560,000 square feet of office space and 100,000 square feet of retail space. A number of the Task Force members, however, as well as residents of the neighborhoods

surrounding Jackson-Taylor, were concerned about the potential increase in traffic and possible impacts on schools, parks and other City services that could be created by the amount of development proposed in Jackson-Taylor. In response to these concerns, a "compromise" reduced density plan, consisting of up to 1,677 residential units, 458,625 square feet of office space and 106,625 square feet of retail space, was presented to the Planning Commission and City Council. This compromise plan was based on the reduced scale alternative analyzed in the Jackson/Taylor Environmental Impact Report and generally meets the objectives developed by the Task Force without risking undue impacts to nearby neighborhoods. The compromise plan received solid support from the community, including several Task Force members, and was approved by the Planning Commission and City Council. This compromise is the foundation of the Residential Strategy.

As a result of amendments to the Residential Strategy initiated in 2008, the density plan was slightly modified to allow up to 2,225 dwelling units. [A/S]

B. The Illustrative Land Use Plan

The Jackson-Taylor Illustrative Land Use Plan recognizes that the existing industrial uses in the area are already undergoing transition and that a land use pattern that maintains jobs, yet is oriented towards providing housing will be more compatible with the surrounding single-family neighborhood. To this end, the plan recommends creating a mixed-use residential and commercial core in the center of the study area that is urban in character and at a scale and density that makes change economically feasible. The surrounding single-family neighborhoods would be buffered from the higher density development by moderate density housing whose design would respect and echo many qualities of the older neighborhoods. Ground floor retail space in selected locations would provide neighborhood-serving and specialty shopping in a manner that complements the Japantown [S] Business District, yet supports employee and resident needs. Public amenities, such as day care and a community center would be located in an expanded Bernal Park. Streets would be opened to re-establish the block pattern of the surrounding area and provide comfortable and pleasant routes for pedestrians. Small parks, connected by these pedestrian streets, would be sited throughout the area to provide relief from the urban environment and to allow public outdoor activities. These elements seek to implement many of the plan's objectives to maintain and enhance the character of the surrounding neighborhood; provide a range of housing types and densities; encourage a mix of housing, employment, shopping and public uses; enhance public open space opportunities; and strengthen pedestrian and street connections within the neighborhood.

The design, configuration and mix of uses is also intended to create a pedestrian-oriented neighborhood that supports the use of public transportation. The plan mixes residential, retail, office, community facilities, parks and nearby schools within a comfortable walking distance, making it convenient for residents and employees to travel by transit, bicycle or foot, as well as by car. A light rail station is located approximately 2,200 feet west of the west edge of the study area, which will permit many new residents to walk to the station. Bus stops within the study area would continue to provide fast and efficient service to the Guadalupe Light Rail Transit Corridor and to other downtown destinations. [S]

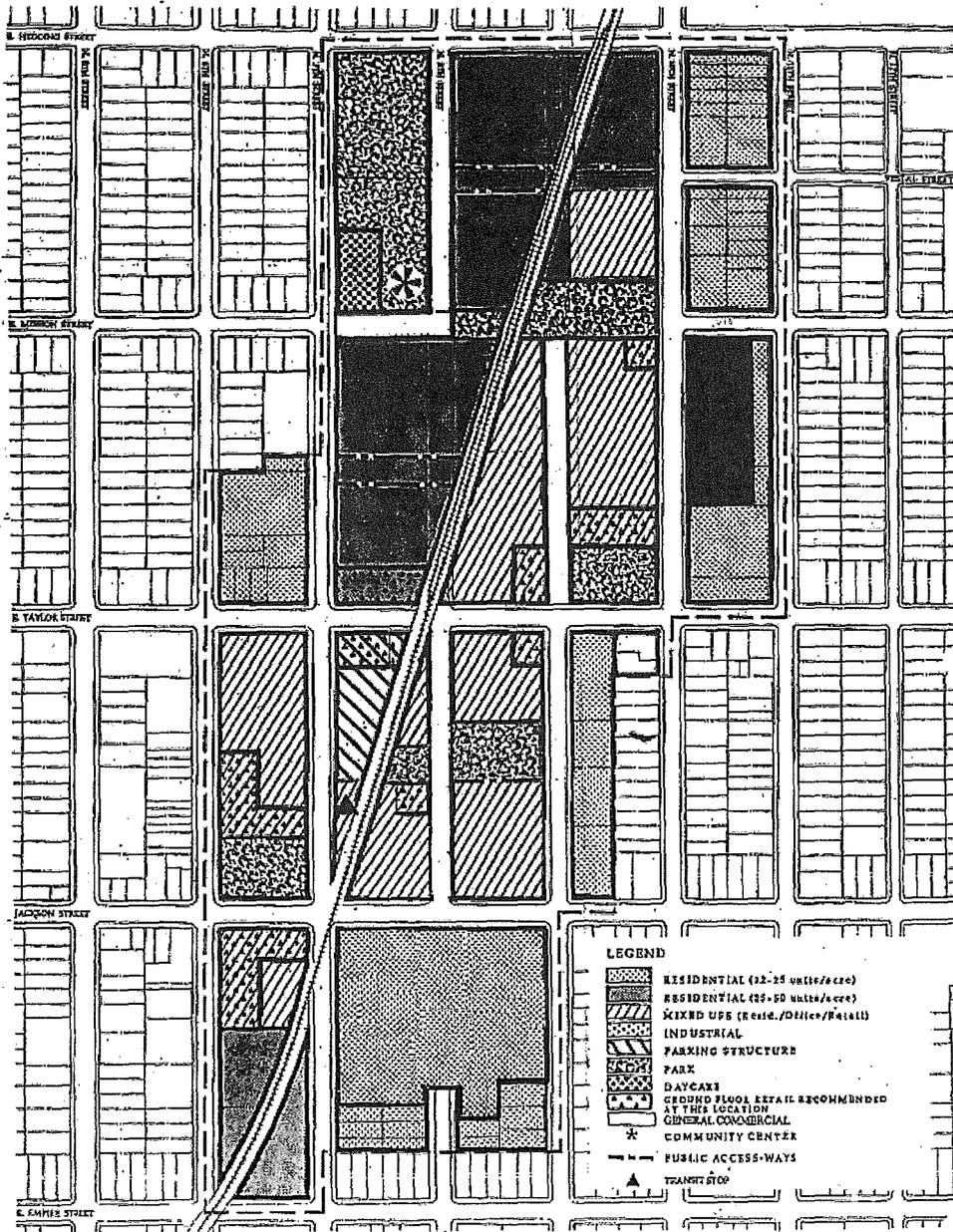
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Deleted: Additionally, BART is considering extending service to San Jose underground along the railroad alignment that passes through the study area. A BART stop is designed into the plan and can be considered a very valuable amenity that would complement the planned mix of uses.

This illustrative land use plan is shown in Figure 5 and represents a preferred land use pattern for the study area. The recommendations of this plan, including street openings/closings, provision of public facilities, and redevelopment of individual parcels, would take place over an extended period of time through small incremental changes. An Illustrative Site Plan, Figure 6, is also provided to illustrate the concepts proposed by this plan and suggest appropriate configuration of buildings and uses. The site plan is in no way binding, but serves as a useful model for future development plans. Table 1 statistically summarizes the development potential of the study area based on the illustrative site plan, but should in no way be construed as exact estimates of residential or commercial space. Additional detail on each element of the plan is provided in Section 7: Policies and Design Standards.

Moderate Density Residential

The 12-25 du/ac residential density, shown on the Illustrative Land Use Plan, is specifically designed to provide a buffer between the existing low density single-family neighborhoods surrounding the site, and the taller buildings of the high density residential and mixed-use core. The character of the surrounding single-family area must be maintained by replicating the lotting pattern of the surrounding area (up to three units permitted on each lot) or at least maintaining the rhythm and street-front relationship of the older residential development on the new 12 - 25 du/ac blocks. Building heights would be a maximum of 40 feet. Additionally, ancillary units are encouraged in this zone, to provide opportunities for mixing affordable ownership and rental housing. This land use designation is suggested for the blocks between 10th and 11th Streets, the western half of 9th Street between Taylor and Jackson, the southern portion of the study area between 7th and 9th streets, and the southern half of the block between 6th and 7th along Taylor street.



ILLUSTRATIVE LAND USE PLAN

CALTHORPE ASSOCIATES
SAN FRANCISCO, CALIFORNIA
BAY AREA ECONOMICS
PUNAJEV, CALIFORNIA

JACKSON - TAYLOR RESIDENTIAL STRATEGY
SAN JOSE, CALIFORNIA

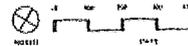


Table 1. Jackson-Taylor Residential Strategy: Estimated Development Potential of Illustrative Site Plan* [S]

Land Use Category	Estimated Acres	Potential d.u./s.f.
PARKS	10 ACRES	
Community Center	Incl. In Bernal Park	10,000 s.f.
Day Care	1.0 acres	Approx. 5,000 s.f.
RESIDENTIAL		
Residential (12-25 du/ac)	17.8 acres	271 units
Residential (12-25 du/ac)	20.7 acres	610 units
MIXED USE		
Residential (25-50 du/ac)		641 units
Office		up to 234,625 s.f.
Retail		up to 56,625 s.f.
Industrial		up to 150,000 s.f.
JAPANTOWN COMPLEX	5.8 acres	
Senior Residential		150 units
Inn		40 rooms
Culture Center		up to 40,000 s.f.
Retail		up to 40,000 s.f.
Commercial		up to 110,000 s.f.
COMMERCIAL	0.4 acres	up to 7,600 s.f.
INDUSTRIAL	7.5 acres	
Mariani/XDM Site		175,000 s.f.
TOTALS		
Residential		1,677 units (including 150 senior units)
Retail		96,625 s.f.
Office		384,625 s.f.
Other Commercial		up to 7,600 s.f.
Industrial		up to 325,000 s.f.
OPTIONS (Not included in above totals)		
Mercado		3,000 s.f.

*These numbers are reflective of the Illustrative Site Plan from the original Jackson-Taylor Residential Strategy, adopted October 6, 1992. [S]

High Density Residential

High density (25-50 du/ac) residential uses would be located adjacent to the mixed-use core south of Hedding along North 10th Street, along North 7th between Mission and Taylor Streets, around the expanded Bernal Park, and at the southwest tip of the study area at the intersection of North 6th and East Empire Streets. This designation permits densities as low as 25 du/ac and as high as 50 du/ac. The intent of this designation is to achieve an average overall density of approximately 35 du/ac. For densities above 35 du/ac, projects should exhibit exemplary architectural design that is urban in character and expresses the essence of the design guidelines contained in the Residential Strategy. Sites selected for this designation are located away from existing single family areas. The high density residential designation can contain a variety of housing unit sizes and types. In fact, a mix of housing densities, types and ownership patterns is desirable within the Jackson-Taylor area to meet all household needs and create a more cosmopolitan community. This building type, is similar, but with fewer stories, to that permitted in the mixed-use core, in that buildings would be up to four stories, generally over a podium of parking and building facades would be varied and articulated to echo the character of the surrounding area. Any high density housing development on the north side of Taylor between 7th and 8th Streets should include ground floor retail. This would allow for the preservation of Bini's Bar and Grill, a "landmark business" in the Jackson-Taylor area.

Mixed-Use Core

The mixed-use core is located in the heart of the study area, extending from East Mission to Jackson Street; north of Taylor Street it is clustered on the east side of the railroad tracks; south of Taylor, it extends from North 6th to North 9th streets. The primary land use designation applied to this area allows a mix of residential, office, and retail uses. New development in this area would be up to 14 stories or 175 feet in height, with some ground floor retail shops, up to two stories of office space, and 1 to 2 stories for community amenity uses. [A] All parking in this area would be below ground; in architecturally or landscape treated structures or structures faced with ground floor commercial, office, or residential space; in interior surface lots surrounded by buildings to hide the parking from pedestrian streets; or along internal private streets. [A] This mix of uses would create a vibrant urban environment that is active during the day and after dark.

Retail uses are identified on the illustrative land use plan within the mixed-use core in strategic locations that provide convenient shopping for residents and employees, as well as frame public plazas and parks. [S] Small retail shops should line these formal public spaces to provide a meeting place for residents and employees to enjoy outdoor lunches and for community activities. Festivals and gathering can also take place here, such as the Farmer's Market, fiestas, and other special events. These activities are not only important to the community, but also help support and bring additional business to the merchants.

Retail space within residential and office areas is also essential to effectively link future transit service with land use. Without these shopping opportunities within convenient walking distance, residents would use cars for greater numbers of trips and workers would lose an additional

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incentive to use transit. Goods and services used on a daily or more-than-once a week basis are especially important to making this transit/land use link.

Limited industrial uses are permitted in existing buildings within the area bounded by Mission, Tenth, Taylor and the railroad tracks. Allowable industrial uses are those which typically might have a retail and/or restaurant component, and which do not generate noise, odor, or other nuisance impacts. For example, allowable uses include the manufacturing of food products, beverages, furniture, and arts and crafts. Auto-related uses are not allowed.

Recognizing the need for additional grocery shopping in the wider area north of downtown and the Hispanic community's desire for a "Mercado," the illustrative plan suggests an option for development on the southern half of the block bounded by Taylor, 10th, 11th, and East Mission. In this location, a small grocery store of up to 30,000 s.f., or series of specialty food shops configured into a mercado, is suggested as an alternative to the identified residential uses.

Office uses are an important component within the mixed-use core, creating the potential for residents to live near their place of employment and allowing workers from around the region to commute to their jobs via transit. Its proximity to on-site retail space and the Japantown [S] Business District permits employees to run errands and have lunch without using a car. An over emphasis on office development could, however, have wider ranging impacts. The amount and type of office development should not compete with downtown, nor should it unduly strain the peak hour capacities of nearby streets and intersections or cause "spillover" parking problems in adjacent residential areas.

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The residential component of the mixed-use core would be a mix of townhomes, condominiums, live/work units and apartments. [A] As further discussed in the Policies and Design Guidelines section, residential units should be a mix of affordable and market rate, with a variety of unit sizes to accommodate singles, couples, seniors and families. [A]

The character of the buildings in the mixed-use core would be varied and articulated to provide visual interest to pedestrians, rather than create an overly unified frontage. Street level windows and numerous building entries would be required for all ground-level office and retail space. Upper stories would be articulated with bays and balconies. Arcades, outdoor seating, and patios are strongly encouraged along the street to provide places for outdoor activities.

General Commercial

The southwest corner of Tenth and Taylor Streets has been designated General Commercial to encourage uses that serve the Jackson-Taylor area and the surrounding neighborhoods. Given the size of the site, small scale retail, personal service shops, and gas station uses are preferred at this corner. All parking would need to be, in an interior or side surface lot to allow the building to be placed close to the sidewalk. The character of any new commercial buildings should provide visual interest to pedestrians with street level windows and attractive entries. An alternate use of High Density Residential (12-25 du/ac) is permitted on this site to provide additional development options.

Industrial

The Mariani Packing Partnership facility, located south of Jackson Street between 7th and 9th streets, was an industrial use that was expected to remain for an extended period of time. However, the site changed use much earlier than anticipated and in 2001, the site was amended to a mix of moderate and high-density housing. [S]

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Deleted: The Mariani Company owns a portion of its site and leases the remainder from XDM Properties. Mariani has indicated that it plans to expand onto the remainder of the XDM site. Recognizing the importance of this existing and planned facility and the number of jobs it provides to residents of the surrounding area, the plan allows on-going operation of this facility. If, however, this individual use is discontinued, its land use designation should be amended to a mix of moderate and high density housing, with retail uses along Jackson Street and Eighth Street should be re-opened to through traffic.

Parks, Open Space, and Community Facilities

Public parks and plazas are fundamental features of livable and enjoyable higher density communities. The plan for the Jackson-Taylor area creates an internal pedestrian network of pleasant walking streets that connect with a series of small public parks and plazas. Bernal Park is also identified for possible expansion from four to eight acres to provide space for a soccer field, daycare facility, and new community center. These parks are shown on the Illustrative Land Use Plan and Illustrative Site Plan in locations that reinforce the pedestrian streets that lead to and from main residential and shopping destinations. Whether designed as a landscaped park or a hard-surface plaza, public spaces should be the focus of commercial and residential developments and be placed next to public streets. For example, a public park is identified in the plan adjacent to the Japantown complex which would link the Japantown [S] Business District with the entry to the mixed-use core along Jackson Street. [S] This site could be developed as either a soft, informal, landscaped space or a formal plaza with a water feature. Its location is important in that it defines an entry to the study area, makes a linkage between old and new, resolves a difficult traffic circulation problem adjacent to the railroad tracks. [S]

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The amount of parks within the study area is based on the City's population-based parkland objectives. For all developments, open space would be required to fulfill both public park and private open space requirements. While all parks and plazas within the study area should be open to the public, some small parks may be dedicated through clustering of private common open space requirements that cannot be accommodated on very high density residential housing sites.

Park site selection also considered the feasibility of implementation and parcel aggregation. In some cases, such as along East Mission Street, a park is sited along the right-of-way of a street that is already closed and only a small amount of additional land would be required from adjacent properties. In other locations, parks are located on parcels that presently contain buildings of poor or marginal quality within a larger site with additional development potential. The park sited in conjunction with the Japantown complex would likely be part of their overall development plan. As further described in the Implementation section, no single property owner would be required to pay for all parks.

Two additional public facilities are identified in the illustrative land use plan, both on the site of the expanded Bernal Park. A one acre site has been set aside for a daycare facility. The size of the site could accommodate care for between 50 and 60 children. A Community Center is also proposed within Bernal Park. While several other community centers presently exist within the broader area, the Hispanic community that lives in the surrounding neighborhood sees a great need for a multi-purpose facility that sponsors activities and has space for classes, meetings, and performances for people of all ages.

Street and Circulation System

The circulation system proposed in the Jackson-Taylor Residential Strategy is based on the following fundamental principles:

- Maintain and/or recreate the block pattern and scale of the surrounding area. Several streets in the study area have been closed to create “superblocks” that are more suitable to the needs of industrial development, than residential or office uses. The plan recommends opening streets in the area to re-establish the fine-grain character of the surrounding area.
- Close streets at strategic locations to make special public plazas. East Mission has already been closed and is proposed as a park site. [S]
- Create a comfortable waking environment by planting street trees to form a continuous canopy at the sidewalk. Primary pedestrian routes within the study area, North 7th, 9th, and Jackson Streets, would be planted with special colorful accent trees.
- Take advantage of existing street widths, whenever possible, to widen sidewalks and providing angled on-street parking.
- Encourage pedestrian and auto traffic to cross the railroad in a safe manner. Special care has been taken to design railroad crossings that are safe for pedestrians and prevent the right-of-way from becoming a narrow isolated, walled corridor.

Deleted: 7th Street north of Jackson Street is proposed for consideration of closure when and if a BART station is constructed there. A final decision on the closure of 7th Street can only be made at the time when detailed BART plans are approved.

7. Policies and Design Guidelines

This section sets forth policies and design guidelines tailored to the conditions of the Jackson-Taylor area in order to assist the City and property owners in designing and building new projects and public improvements. They aim to implement the most important urban design principles of the land use plan, yet be flexible enough to provide opportunities for new, innovative site plans and building designs.

These policies and design guidelines are in no way exhaustive and should be considered a supplement to existing City policies and the adopted residential and commercial design guidelines.² The following policies and design guidelines should be applied to proposed projects at the zoning and development permit stages.

This section is organized as follows: general guidelines that apply to the entire study area; guidelines pertaining to the individual land use designations; street and parking policies and guidelines; and finally, transit policies.

A. Overall Policies

The following policies address issues that affect the entire Jackson-Taylor study area and apply to all future projects.

- The Residential Strategy provides opportunities for a mix of higher density housing types; these opportunities should be utilized to produce housing for all income, age and ethnic groups in a mix reflecting the demographics of the surrounding area.
- Each housing type should contain a mix of unit sizes to accommodate different types of households (e.g. singles, families, seniors, mixed-income housing, etc.) [A]
- Housing types should provide opportunities for ownership and rental housing. To the extent feasible, ownership housing should be encouraged over rental housing for all income levels.
- In order to promote some degree of self-sufficiency within the Jackson-Taylor area, the Residential Strategy illustrative land use plan includes some employment generating and neighborhood-serving commercial uses. Mixed-use areas should develop consistent with this principle of [S] the illustrative land use plan in order to achieve both the housing and employment objectives of the strategy.
- In order to help preserve some of this area's historic character, older buildings which are remnants of the food processing industry and have some architectural merit (i.e. the Del Monte and Continental Can buildings) should be preserved and incorporated into new projects. Where buildings are preserved, minimum intensity or density requirements could be waived.

² "Towards Community. Residential Design Guidelines for the City of San José." Department of Planning, Building and Code Enforcement, November 1988, revised May 2008, and "Commercial Design Guidelines," Department of Planning, Building and Code Enforcement, May 1990. [S]

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- To the extent feasible, new construction should preserve viewsheds and view corridors of the east foothills and downtown.

B. Land Use Policies and Design Guidelines

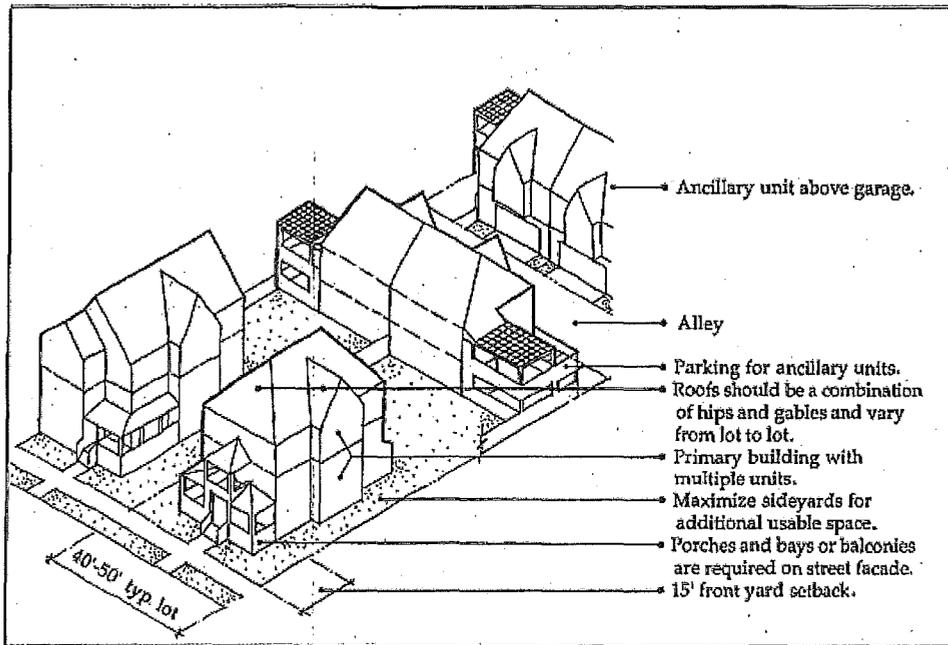
12-25 du/ac Residential

The following is a discussion of policies and design guidelines for all sites within the 12-25 units/acre residential designation. The design guidelines are also illustrated in Figure 7.

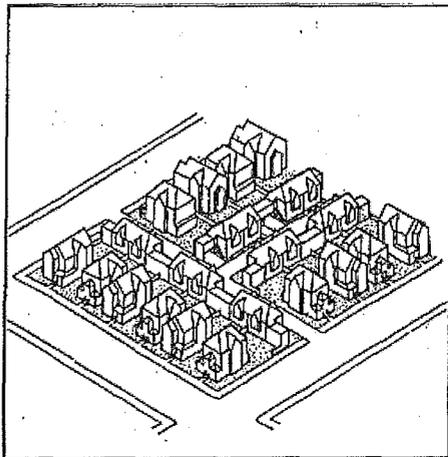
Density and Building Types

- Buildings within this zone may be a maximum of three stories, not to exceed 40 feet in height. A 15 to 20 foot front yard setback is required; porches, bays and balconies may extend into the front yard setback.
- The lotting pattern of the site should be comparable to the lotting pattern of the surrounding single family area. Lots may be between 35 and 60 feet wide and at least 100 feet deep, however, lots with widths of between 40 and 50 feet are preferred. As an alternative, larger scale projects with or without individual lots may be allowed, but only if they can replicated the rhythm and street-front relationships of the preferred individual lot pattern.
- A minimum density of 12 units/acre is required; a maximum density of 25 units/acre is permitted, provided these densities are accommodated in the lotting pattern and building envelop described above and all other City open space and parking requirements are met.
- Each lot should have a single primary building, with up to two internal units and at least one side yard. The 3-unit configuration is strongly encouraged on lots 45 feet and wider; the 2-unit configuration is allowed on narrower lots. Zero-lot-line configurations are recommended to provide a single, but wider side yard.
- A mix of ownership and rental housing should be provided in this area and is encouraged on a parcel-by-parcel basis.
- Ancillary "second" units located above the garage are encouraged in this area and should be counted in the density calculation.
- Garages must be located at the rear of individual lots consistent with the traditional pattern in the surrounding area. Alleyways are strongly encouraged to eliminate on-street curb cuts and for easier access to garages at the rear of the property. Limiting the number of on-street curb cuts helps to facilitate the flow of traffic and safety on heavily traveled [S] streets.

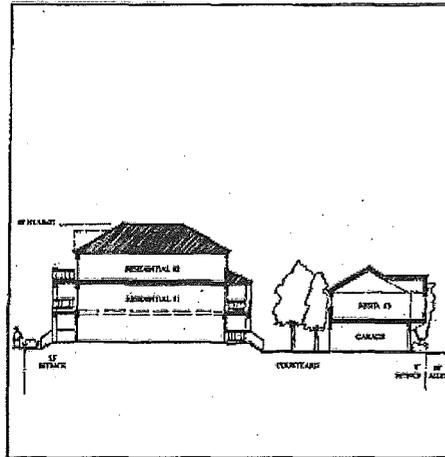
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Guidelines



Typical Block Axonometric



Typical Section

DESIGN GUIDELINES
Residential (12-25 du/acre)

CALTHORPE ASSOCIATES
SAN FRANCISCO, CALIFORNIA
BAY AREA ECONOMICS
EMERYVILLE, CALIFORNIA

JACKSON - TAYLOR RESIDENTIAL STRATEGY
SAN JOSE, CALIFORNIA

FIGURE 7

Architectural Guidelines

- Porches, bays and balconies are required on all street facades to reflect the rhythm and character of the surrounding single-family residential area. Porches should have a minimum depth of 7 feet, cover a minimum of 50% of the width of the front facade and should have a minimum front setback of 8 feet. For bays and balconies, a minimum front yard setback of 10 feet is required.
- Variation in building facades should be achieved, in part, by using a variety of materials along each street, including stucco, wood siding, stone, and brick. In general, high quality materials are encouraged; pre-fabricated and non-durable materials are discouraged.
- Entries to the primary building must be provided from the ground floor street-facing facade; secondary entries may be provided from alleys, garages, and parking lots. Exterior stairs to upper floor units are not permitted on street facing facades, including the front portions of side elevations. Ancillary units may be accessed from rear yard or alley entries.
- Roofs should vary from lot to lot, with a combination of gable and hip roofs.

25-50 du/ac Residential

The following is a discussion of policies and design guidelines for all sites within the-25-50 units/acre residential designation. The design guidelines are also illustrated in Figure 8.

Density and Building Types

- Buildings within this zone may be a maximum of four stories over parking, not to exceed 45 feet in height. Any parking podiums should be depressed so that first floor residential units are not more than 5 feet from finish grade. A 15 foot front yard setback is required for the podium and building; porches, bays and balconies may extend into the front yard setback.
- A minimum density of 25 du/ac is required, a maximum density of 50 du/ac is permitted, provided these densities are accommodated in the building envelop described above and all other City open space and parking requirements are met. For densities above 35 du/ac, projects must exhibit exemplary architectural design that is urban in-character and expresses the essence of the design guidelines contained in the Residential Strategy.
- A mix of unit sizes and types should be provided in each residential complex to provide opportunities for households of a variety of sizes and income levels.

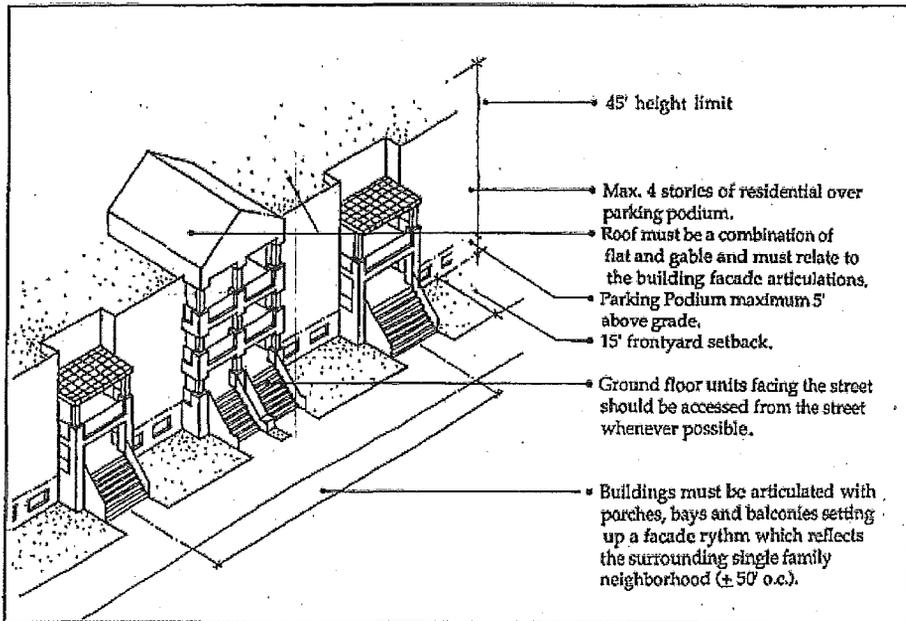
Architectural Guidelines

- Building facades should be varied and articulated to provide visual interest to pedestrians, rather than create an overly unified frontage. Building massing should be articulated with porches, bays, and balconies placed 50 feet on center to reflect the rhythm of the

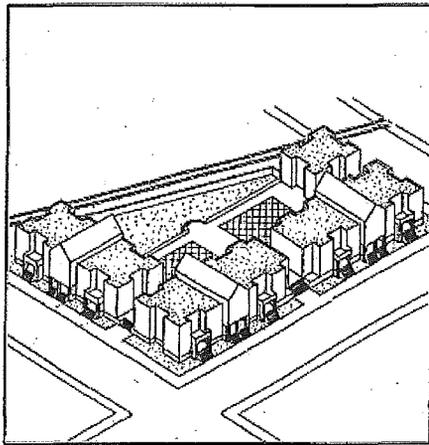
surrounding single family residential area: Porches may extend up to 7 feet into the front setback; bays and balconies may extend 5 feet into the setback. In no case shall the street facade of a building consist of an unarticulated blank wall or an unbroken series of garage doors.

- Building openings should face the street and sidewalk to enliven the pedestrian environment by providing a pleasant sense of connection between indoor and outdoor activities. Primary entrances must orient to streets, plazas, or parks; not to the interior of blocks or to parking garages. Street access to residential units should be frequent, at approximately every 100 feet. First floor units facing the street should be accessed directly from the street whenever possible. Exterior stairs to upper floor units are not permitted on street facing facades, including the front portions of side elevations.
- Architectural diversity is encouraged; large scale projects diminish the liveliness of the street by their sameness of style and detail. No building or project should appear to dominate an entire street or block. Variations in floor level, facades, roof styles, architectural details, and finishes that create the appearance of several smaller projects are encouraged. Street elevations should also be broken with reveals, recesses, and other architectural features to provide visual interest.
- Building materials such as concrete, stucco, masonry, tile, stone, and wood are encouraged. Variation in building facades should be achieved, in part, by using a variety of material along each street. In general, high quality materials are encouraged; pre-fabricated and non-durable material is [S] discouraged.
- Roof lines should be articulated and relate to the facade articulation and massing of the building. Roofs should be a combination of flat and gable. To achieve variety in design, a single roof style should not be used for an entire project, unless the project is clearly too small to accommodate such variety.
- Street-facing garage entrances should be as narrow as is consistent with safety and should be spaced at least 200 feet apart.
- All vents from the street to the parking garage must be screened with louvers, screen walls, or porches; planting is not an alternative to screening material. All parking garage lighting should be shielded so that light does not shine through vents at night. If forced venting is required for the garage, air should not vent directly onto the sidewalk.
- Double-aspect units (units that extend from the front to the rear of the building) are preferred over double loaded corridor buildings, where feasible.
- Outdoor plazas over the parking podium should be landscaped to provide both common and private open space. This space should be designed as an intimate courtyard.

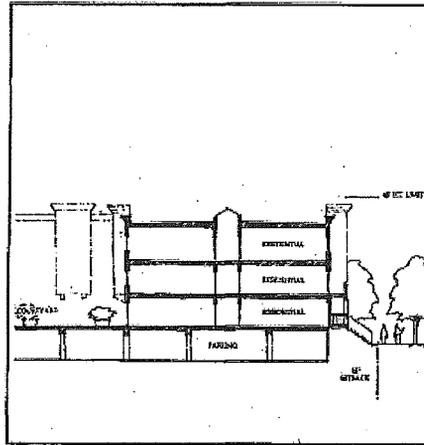
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Guidelines



Typical Block Axonometric



Typical Section

DESIGN GUIDELINES
Residential (25-50 du/acre)

CALTHORPE ASSOCIATES
SAN FRANCISCO, CALIFORNIA
BAY AREA ECONOMICS
RESEARCH, CALIFORNIA

JACKSON - TAYLOR RESIDENTIAL STRATEGY
SAN JOSE, CALIFORNIA

FIGURE 8

- Ground level open space (not over parking podium) should be provided for each building or project. This outdoor amenity may be partially satisfied within a nearby pocket park. Ground level open space should be planted with trees at a minimum. Seating, tables and children's play equipment are encouraged.

Parking

- Parking garages should not dominate the frontage of a street or prohibit pedestrian movement. Structured parking areas should be depressed at least 1/2 level below the ground surface and must be below the residential component of the project to minimize impact on the building facade and street frontage.
- All street-facing garage entrances should be spaced at least 200 feet apart.
- Where structured parking is not feasible, surface parking areas should follow the parking design guidelines contained in the City of San José's "Residential Design Guidelines".

Mixed Use (Residential, Office, Retail, and Industrial)

The following is a discussion of policies and design guidelines for all sites within the Mixed Use (residential, office, retail) designation. Key design guidelines are also illustrated in Figure 9.

Density/Intensity and Building Types

- Buildings within this zone other than the Japantown Complex may be a maximum of 6 stories, not to exceed 65 feet in height from finished grade.³ [A] Buildings within the Japantown Complex may be a maximum of 175 feet in height from finished grade, except for buildings across from the historic district on the southern half of 6th Street, which may be a maximum of 85 feet in height from finished grade. [A] The parking podium must be interior to the building and be surrounded by commercial uses, office, or residential uses; or be architecturally or landscape treated; up to one sub-surface level of parking is permitted. Front and side yard setbacks are as follows: retail uses, including bays and arcades, may extend to the property line; and office uses may be built-to within five feet of the property line. [A] In all cases, upper floors of the building should, at a minimum, follow the setback of the ground floor use, however, stepped back facades are encouraged.
- Except for buildings within the Japantown Complex that front on Taylor Street or on 7th Street [A] and the 3.24-acre parcel on the west side of North 10th Street, between Vestal Street and East Mission Street [S], a mix of residential and commercial space is required in all buildings containing residential uses. The commercial component of the mixed-use building may develop to an equivalent FAR of 0.8 at a maximum of two stories. [A] The

Deleted: ; if the site across the street is designated for the 12-25 du/ac residential type, a 15' foot setback is required.

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Deleted: ; retail shops should be located at identified sites (see below).

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³ The building height limits are to be measured to the highest point of the coping of a flat roof, or to the deck line of a mansard roof, or to the average height of the highest gable of a pitched or hipped roof; measurements must be consistent with all other City of San José codes. This height limit was determined based on construction techniques allowed by the Uniform Building Code; higher buildings would necessitate exclusive use of concrete and steel materials, thus increasing construction costs and reducing the affordability of residential units. Type IV construction techniques are preferred which allow up to two floors of concrete/steel construction for the commercial component, and up to four floors of heavy timber wood frame construction for the residential component.

residential component must develop to a minimum of 25 du/ac and may be a maximum density of 50 du/ac. [S] For the 3.24-acre parcel on the west side of North 10th Street, between Vestal Street and East Mission Street, the maximum density is 60 du/ac. [S] The Japantown Complex may contain up to 600 dwelling units. Across the entirety of the Japantown Complex area, the maximum density is anticipated to be 160 du/ac, based on 600 units and current net acreage assumptions, provided that densities on individual parcels within the Japantown Complex may exceed this maximum. [A] For densities above 35 du/ac, projects must exhibit exemplary architectural design that is urban in character and expresses the essence of the design guidelines contained in the Residential Strategy. The ground floor level of all buildings in this zone should include some commercial uses, provided that within the Japantown Complex this requirement applies only along Jackson Street and 6th Street [A]; upper floors may be either all residential or may include one additional floor of office space. For the 3.24-acre parcel on the west side of North 10th Street, between Vestal Street and East Mission Street, no commercial component is required. [S] These intensities/densities must be accommodated in the building envelope described above and must meet all other City open space and parking requirements.

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- Limited industrial uses are allowed only within existing buildings located between Mission, Tenth, Taylor, and the railroad tracks. Parking must be adequately screened.

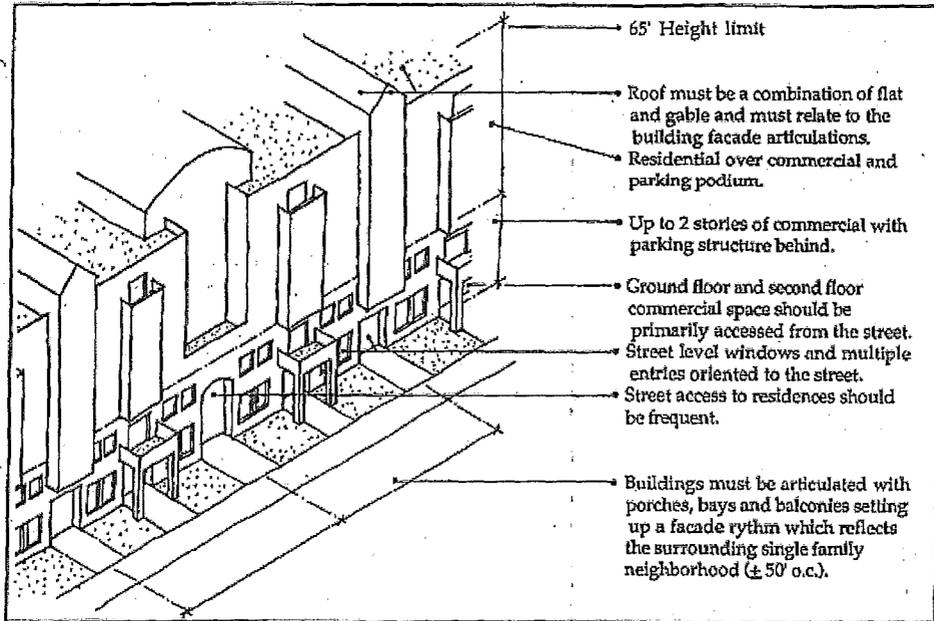
Architectural Guidelines

- Building facades should be varied and articulated to provide visual interest to pedestrians, rather than create an overly unified frontage. Street level windows, display bays, and individual tenant building entries are required for all ground-level office and retail. Upper stories should be articulated with porches, bays, and balconies placed 50 feet on center to reflect the rhythm of the surrounding single family residential area; a finer, 25 foot articulation is encouraged. Bays and balconies may extend 5 feet into the setback. Arcades, outdoor seating, and patios are strongly encouraged along the street to provide places for activities. In no case shall the street facade of a building consist of an unarticulated blank wall, a parking garage, or an unbroken series of garage doors, with the exception of the former Corporation Yard site, where an exposed parking garage may be considered. [A]
- Building entries should face the street and sidewalk to enliven the pedestrian environment by providing a pleasant sense of connection between indoor and outdoor activities. Ground floor and second floor spaces should be primarily accessed from the street, rather than from internal corridors; exterior stairs to upper floor units are not permitted on street facing facades including the front portions of side elevations. Primary entrances must orient to streets, plazas, or parks; not to the interior of blocks or to parking garages; secondary building entries may be provided from parking lots or structures. Street access to upper residential units should be frequent, at approximately every 100 feet.
- Architectural diversity is encouraged; large scale projects diminish the liveliness of the street by their sameness of style and detail. No building or project should appear to dominate an entire street or block. Variations in floor level, facades, architectural details,

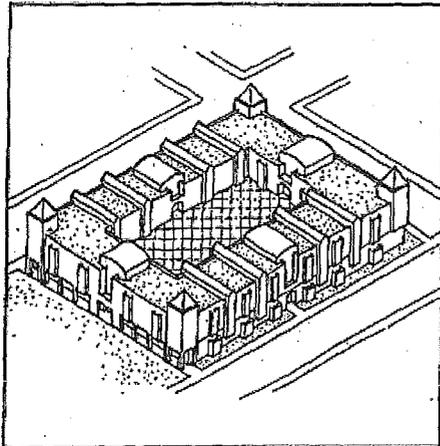
and finishes that create the appearance of several smaller projects are encouraged. Street elevations should also be broken with reveals, recesses, and other architectural features to provide visual interest.

- Building materials such as concrete, stucco, masonry, tile, stone, and wood should be encouraged; with the exception of the former Corporation Yard site, glass curtain walls and reflective glass will be discouraged. [A] Variation in building facades should be achieved, in part, by using a variety of material along each street. In general, high quality materials are encouraged; pre-fabricated and inexpensive material is, [S] discouraged.
- Roof lines should be articulated and relate to the facade articulation and massing of the building. Roofs should be a combination of flat and gable. To achieve variety in design, a single roof style should not be used for an entire project, unless the project is clearly too small to accommodate such variety.

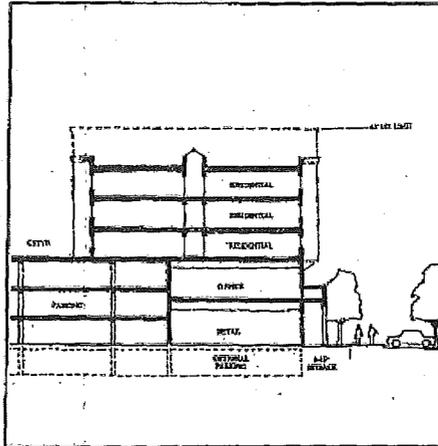
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Guidelines



Typical Block Axonometric



Typical Section

DESIGN GUIDELINES
(Mixed Use)

CALTHORPE ASSOCIATES
SAN FRANCISCO, CALIFORNIA
BAY AREA ECONOMICS
BERKELEY, CALIFORNIA

JACKSON - TAYLOR RESIDENTIAL STRATEGY
SAN JOSE, CALIFORNIA
FIGURE 9

Residential Guidelines

- A mix of unit sizes and types should be provided in the residential component of each building to provide opportunities for households of a variety of sizes and income levels.
- Double-aspect units (units that extend from the front to the rear of the building) are preferred over double loaded corridor buildings, where feasible, with the exception of the former Corporation Yard site. [A]
- Outdoor plazas over the parking podium should be landscaped to provide both common and private open space. This space should be designed as an intimate courtyard.
- Development should be consistent with applicable residential design guidelines. [S]

Retail Guidelines

- The Illustrative Land Use Plan identifies several locations for retail space. These spaces must be developed in the ground floor level of vertically mixed-use buildings, [A]
- In addition to the general design guidelines for all mixed-use buildings, retail space must orient to the street or to public parks and plazas, rather than to internal parking lots or “internal streets.” Retail space may orient to “internal streets” within the Japantown Complex. [A]
- Display windows, recessed or distinctive entries, and awnings, are strongly encouraged in retail areas. Variety in storefront facade treatment, materials, awnings, and signage is encouraged from building to building, and to a lesser extent for each tenant. Signs should be integrated into the overall design of the building facade. They may be attached to the face of buildings, canopies or awnings. Window signage should be minimized to maximize visual access between indoors and outdoors. Internally lit (not including neon), computerized, or moving signage is not permitted.

Appropriate Uses in Retail Spaces

Bakeries
Banks
Book stores
Camera store
Clothing stores
Collectables Shops
Daycare
Delis
Drugstores
Dry cleaners
Florists
Food, grocery stores
Art and Craft Galleries

Gift stores
Hardware Stores
Health club, gyms
Home furnishings
Ice cream stores
Instruction studios

Deleted: The identified locations for ground floor retail space are fixed, unless the City deems that the modification does not substantially change the intent of the plan

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Laundromats	Shoe stores
Office supplies	Small appliance. repair
Personal service shops	Small theater
Pet stores	Specialty Foods
Post office	Sporting goods
Professional offices	Stationery stores
Public/ government uses	Tailor
Radio, TV; video, and music stores	Toy stores
Restaurants, bars	Variety stores
Schools-commercial	

In addition to the previous list of uses, any other retail or service use is allowed which is compatible with the surrounding neighborhood and is intended to provide services to neighborhood residents.

Office Guidelines

- Within this designation, professional and business offices, as well as medical clinics, are permitted.
- The amount of office space in this mixed-use designation has been intentionally limited to two, stories in order to recognize a “residential emphasis” in the Jackson-Taylor area and to minimize peak hour traffic congestion on nearby streets.
- Public and semi-private plazas are encouraged in the design of office uses in addition to the parks and plazas identified on the Illustrative Land Use Plan. These plazas can create elegant entries and places to relax outdoors. Plazas should be visually and physically connected to public streets.

Industrial Guidelines

- Limited industrial uses are allowed only within existing buildings located between Mission, Tenth, Taylor, and the railroad tracks.
- Allowable industrial uses are those that typically might have a retail and/or restaurant component, such as the manufacturing of food products, beverages, furniture, and arts and crafts. These industrial uses cannot generate noise, odor, or other nuisance impacts.
- No auto-related uses are allowed.
- Attractive landscaping, fencing, and other screening is required. Architectural embellishments which result in an improved building appearance are encouraged (e.g., awnings, changes to roof lines, etc.).
- Retail and/or restaurant components should be easily visible and accessible from the street without creating conflicts between customers and the industrial operations.

Parking

- Parking lots or structures may not dominate the frontage of a street or prohibit pedestrian movement. Parking lots should be located in the interior of a block and be “wrapped” by commercial, office, or residential uses, or architecturally or landscape treated. [A]
- Street-facing garage entrances should be spaced at least 200 feet apart.
- Alternate use parking allowances are strongly encouraged, particularly for joint commercial and residential guest parking spaces. Resident parking should be separated from the office and retail parking.

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Special Conditions

- The Japantown complex is being proposed on the City’s existing Corporation Yard for a mixed-use development that would include residential and retail uses, in addition to a park and community amenity space. [A] The following design guidelines should be applied to this project to ensure that it is compatible with the surrounding neighborhood and new improvements within the study area [A]:

Deleted: This complex is likely to consist of a cultural and performing arts center, plazas, markets, senior residential, an inn and conference center, and retail and commercial spaces.

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- 1) Certain elements of the Japantown complex may be located to adjacent blocks.
- 2) First floor building spaces along streets should orient to the street, and first floor building spaces along Jackson Street and 6th Street should be occupied primarily by retail uses. [A] Retail spaces, however, may also be accessed through interior garden spaces.

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[A] To reduce the need for extensive underground parking at the City Corporation Yard site, [A], and to provide adequate public parking for both the Japantown Complex and other nearby retail uses, a four story parking structure may be developed in conjunction with this project on the triangular-shaped block located directly east of the site. The parking structure associated with the Japantown complex may face onto the street, and is exempt from the above mentioned guidelines regarding parking garages on the street. The northern frontage of this parking garage (Taylor Street) should be developed with ground floor retail uses. [S] To minimize traffic circulation problems, the main entry to this structure should be at its southern end next to the proposed park/transit stop site. The location of the entry must be coordinated with the potential street closure to ensure that adequate space for turning movements is provided.

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- The southern portion of the block between 10th and 11th on Taylor may be developed as a grocery store or “mercado” with up to 30,000 square feet of retail space and surface parking. If this option is pursued, site design should bring the building to the southern end of the block to line the street frontage. Figure 10 illustrates a recommended site plan for this option.

Deleted: ; the southern tip of the block should be reserved for a BART station.

- Due to the irregular shape of the parcel, the site at the southeast corner of Jackson and 6th may develop with ground floor retail along both Jackson and North 6th Streets, residential above, and surface parking behind the building. The amount of retail space and density of residential uses will be limited by the amount of surface parking that can be provided.

General Commercial

- The only site on which this designation applies is the southwest corner of Tenth and Taylor Streets. The retail and office guidelines articulated under the Mixed Use designation also pertain to the General Commercial designation. In addition, gasoline service stations are a permitted use within the General Commercial designation.
- As further discussed in the Implementation section of this report, the southwest corner of Tenth and Taylor Streets has an alternate use of High Density Residential (12-25 du/ac). This provides additional development options for this site.

[S] [S] [S]

C. Parks, Open Space and Public Facilities Policies and Design Guidelines

Parks

- The location of [S] parks identified on the Illustrative Land Use map should be maintained as much as feasible in order to implement the concept of pedestrian connections to and from major destinations in the study area. With the exception of the Bernal Park expansion, [S] modifications to park locations would be considered as long as open space objectives can still be achieved, the minimum size of the parks [S] are [S] maintained, and the new location serves the same housing developments as the original park sites [S].
- The idea of creating a soccer field on the expanded Bernal Park is strongly recommended by the community and should be explored by the City. The feasibility of providing additional tennis and picnic facilities should also be examined.
- Small park sites within the study area (other than Bernal Park) may be developed either as formal "hardscape" plazas or landscaped parks. Formal plazas should center on a monument, sculpture or small fountain. With the exception of the former Corporation Yard site, [A] landscaped parks adjacent to residential uses should provide play areas for children. Seating and picnic tables are encouraged.
- Parks and plazas should provide adequate shading for comfortable mid-day summer use and sunny areas for winter use. Trees must, at a minimum, be located around the perimeter of these parks/plazas; shade trees with some flowering accent trees are encouraged. Landscape design must respect vistas created by streets.
- All buildings adjacent to parks must front on and have entries that face the parks.

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- The only site on which this designation applies is the Mariani Packing Partnership and XDM parcels located south of Jackson Street between 7th and 9th. Mariani has indicated it plans to expand onto the XDM property. If it does not, the XDM land use should be 12-25 du/ac.

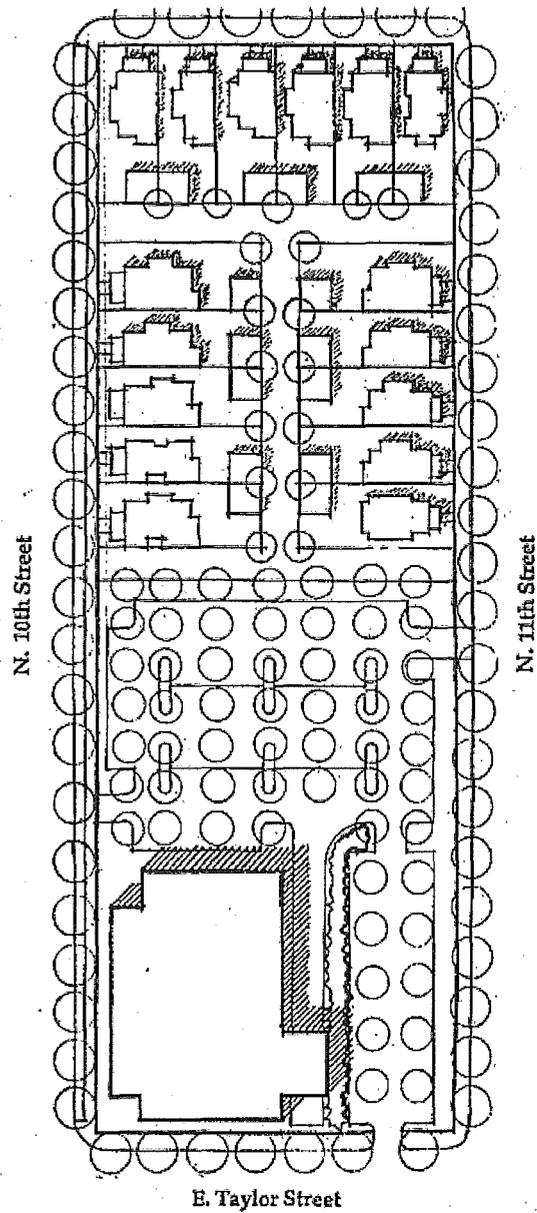
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- As further discussed in the Implementation section of this report, the Mariani Packing Partnership, or another industrial use with similar activity patterns and a comparable number of jobs, may continue to operate at this location. In no case should a subsequent industrial use impose on the surrounding neighborhood adverse impacts greater than the Mariani operation.

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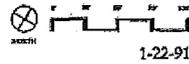


Retail (30,000 s.f. Grocery)
Alternative for Block @ E. Taylor between N.10th & N. 11th Street

CALTHORPE ASSOCIATES
SAN FRANCISCO, CALIFORNIA
BAY AREA ECONOMIC
DEVELOPMENT, CALIFORNIA

JACKSON - TAYLOR RESIDENTIAL STRATEGY
SAN JOSE, CALIFORNIA

FIGURE 10



Day Care

- A one-acre [S] site has been identified in the southeast corner of the expanded Bernal Park for a day care facility. This facility could serve pre-school children, an after-school latch-key program, or both.
- Site design should provide both unstructured outdoor space and a secure playground for the sole use of the day care center.
- As with other commercial and residential buildings in the study area, the day care facility should have ground floor windows, a prominent entry that faces the street, and architectural styling that is distinctive, yet compatible with the surrounding area. A height of at least 2 stories is encouraged. A vertical architectural element, such as a tower or cupola, could be used to signify the special purpose of the building.

Community Center

- A site for a 10,000 square-foot [S] Community Center is identified within the expanded Bernal Park, adjacent to the proposed Day Care facility.
- The facilities within the Community Center should accommodate activities for all ages and should include a large indoor hall suitable for meetings, dances, plays, classes, and other community activities.
- The architectural style of the building should have ground floor windows, entries that faces the street and the park, and architectural styling that is distinctive, yet compatible with the day care center and the surrounding area. A height of at least 2 stories is encouraged. A vertical architectural element, such as a tower or cupola, could be used to signify the special purpose of the building.

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D. Streets and Parking Policies and Design Guidelines.

The following is a discussion of policies and design guidelines for all streets within the study area. Figure 11 is a key to the street types and conditions that are illustrated in Figure 12.

Re-Opening/Closing Streets

A primary design principle of the Jackson-Taylor Residential Strategy is to re-establish the fine grain block pattern of the surrounding area and create additional access for pedestrians. To this end, the plan makes recommendations regarding street openings and closures.

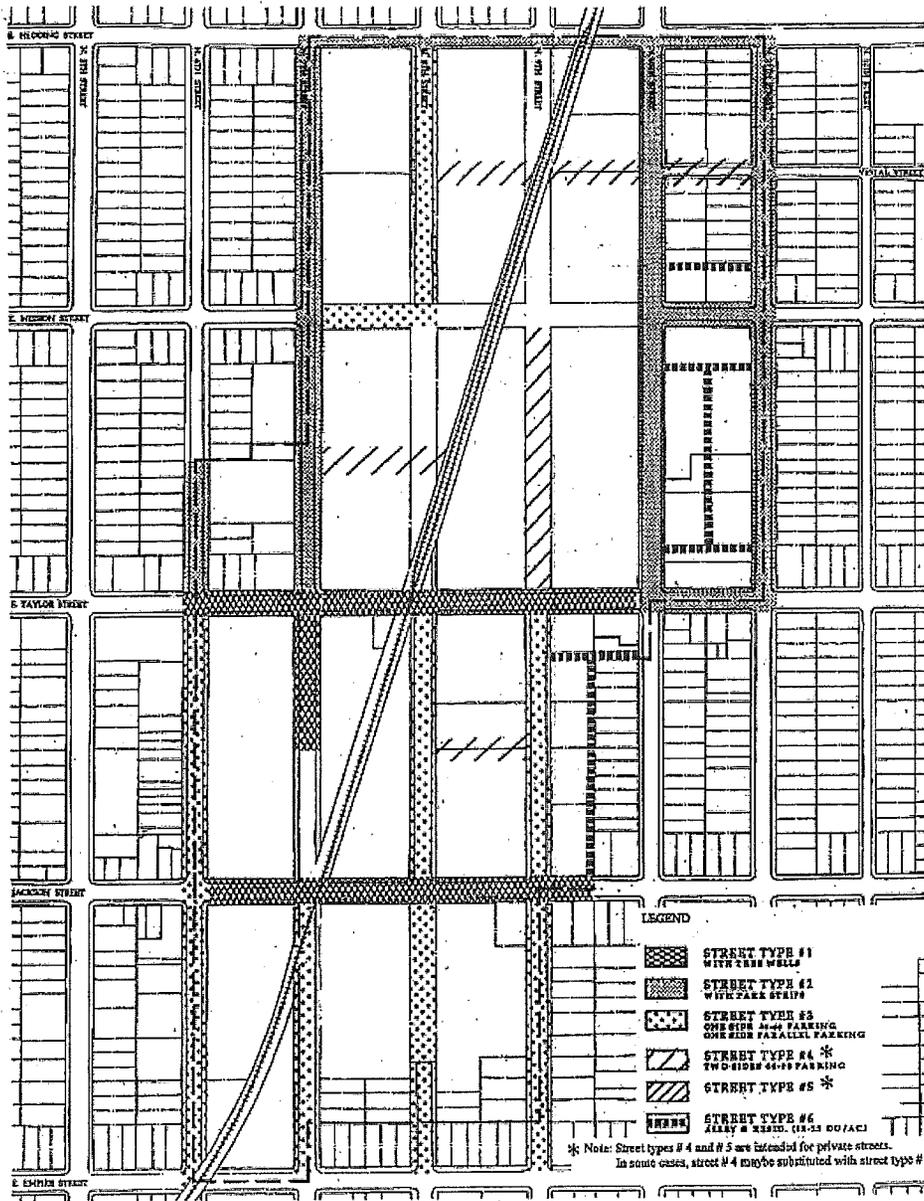
- Two streets within the study area have been identified for "re-opening" at the time that development occurs on adjacent parcels. They are:
 - East Mission between North 7th and 8th
 - North 9th between East Mission and Taylor

If it is not practical to re-establish these streets as public streets (street types #1, #2, or #3) they should be designed as private “parking streets” (street type #4). In particular, North 9th between Mission and Taylor must be a public street if there are long term, limited industrial uses present on one side of the street.

- New Type #4 streets are proposed in three locations:
 - 1/2 block between Redding and East Mission from North 8th to 9th, a pedestrian connection across the railroad tracks, and through to 10th Street.
 - 1/2 block between East Mission and Taylor from North 7th to 8th Streets.
 - 1/2 block between North 8th and 9th between Taylor and Jackson Streets..:

[S]

Deleted: The northern leg, of the Jackson/7th Street intersection should be closed in conjunction with the proposed BART stop. This is strongly recommended as a means to eliminate a highly irregular-shaped intersection, smooth traffic flow in the area, and ensure adequate pedestrian safety adjacent to both the railroad and a new retail center (Japantown).

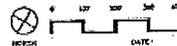


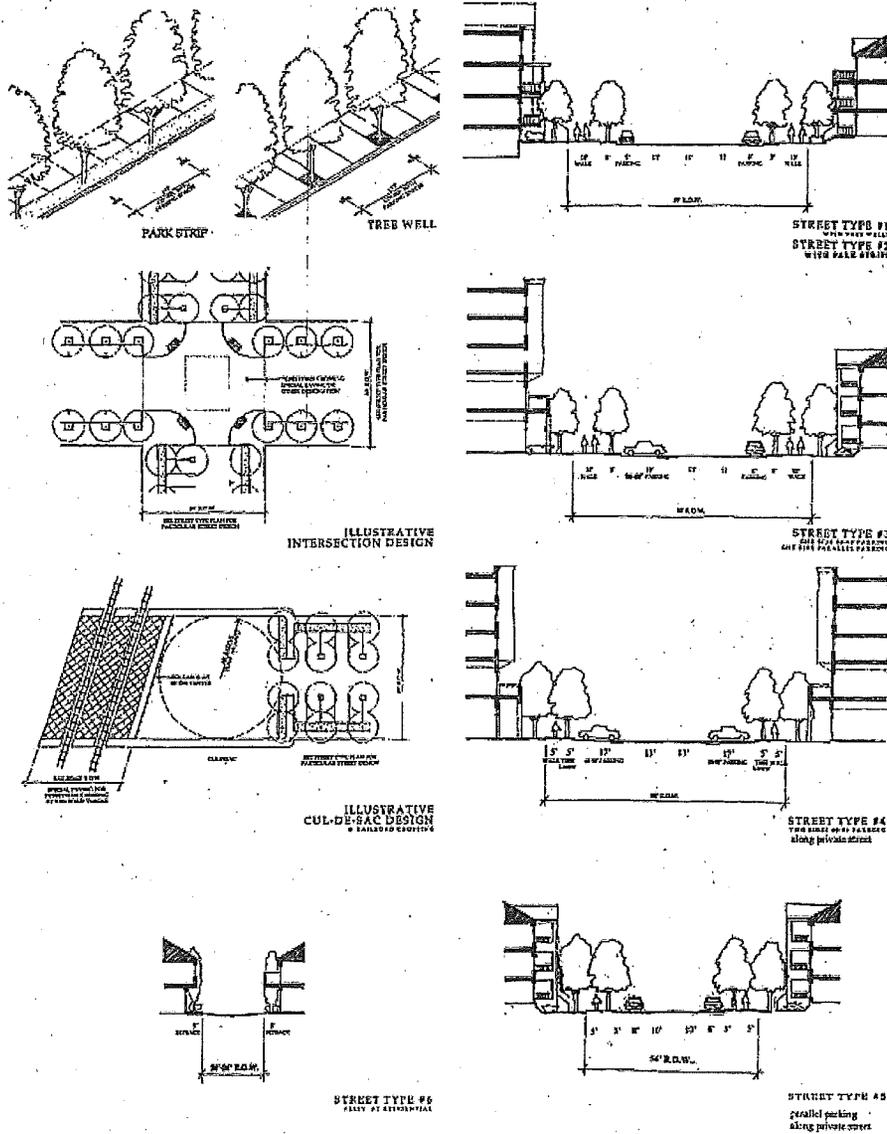
STREET TYPES PLAN

JACKSON - TAYLOR RESIDENTIAL STRATEGY
SAN JOSE, CALIFORNIA

FIGURE 11

CALTHORPE ASSOCIATES
SAN FRANCISCO, CALIFORNIA
BAY AREA ECONOMICS
SHELLEY, CALIFORNIA





STREET TYPES AND ILLUSTRATIVE DETAILS

JACKSON - TAYLOR RESIDENTIAL STRATEGY
SAN JOSE, CALIFORNIA

FIGURE 12

Street Landscaping and Reconfiguration

- Street trees and their canopies protect pedestrians from sun and wind, and provide a pleasant sense of enclosure. Street trees must be planted to form a continuous canopy at the sidewalk (approx. 25 ft. (on center [S]); at angled parking, trees should be planted between every 3 to 4 spaces.
- Special pedestrian streets, such as Jackson, 7th, and 9th streets, and gateways to the area should be given special landscaping treatment, such as colorful, flowering trees. A consistent tree species should be planted along the length of these streets. Accents of flowering trees and deciduous trees are encouraged to highlight changing seasons, and to provide summer shade and winter sun.
- Short cuts through parks, plazas and greens should be encouraged and integrated into the design of spaces. Alternate paths should be provided around parks for night use.
- On-street parking should be maximized throughout the area. Angled (30 to 60 degree) parking is preferred along Mixed Use and Residential (25 to 50 units/acre) streets, and parallel parking along Residential (12 to 25 units/acre). See Figure 11 for specific locations.
- Sidewalks should be a minimum of 5 feet wide and preferably 10 feet wide for more heavily traveled [S] areas. See Figures 11 and 12 for specific locations.
- Intersections should be designed to minimize pedestrian crossing distances and slow traffic.
- Entries to alleys should be designed as driveway cuts, rather than full curb returns; signage should indicate that through traffic is not permitted.

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Railroad Rights-of-Way

- Pedestrian railroad crossings are identified at East Mission, and at East Taylor. Care should be taken to provide adequate siting distances. Special paving across the tracks will help keep the walking surface even.
- Where parks are adjacent to railroad tracks, 8 foot high fences or walls should be placed next to the tracks to prevent children or play equipment from going into the tracks. Secure, but visually open fences of durable design are preferred over concrete walls to keep the railroad corridor as open as possible.

E. Transit Policies and Design Guidelines

- Transit stops constitute an important public use and civic focal point in neighborhoods. [S] To encourage transit use, shelter, convenient passenger loading zones and secure bike storage should be provided at a minimum. Shelters should be easily recognized, yet be integrated with the surrounding architecture and landscaping.

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- Newspaper stands, vending machines bicycle storage, public phones and other elements should be incorporated into the overall design of the transit stop site.

8. Implementation Strategies

A. General Plan

The Jackson-Taylor Residential Strategy is consistent with the San José 2020 [S] General Plan. The General Plan establishes the land use pattern and major development policies for the study area in the Jackson-Taylor Planned Residential Community (PRC). The Jackson-Taylor PRC contains the major features of the Residential Strategy. The Jackson-Taylor Residential Strategy is a separate policy document, providing the background, vision, and community character for the PRC and also a level of detail for implementation beyond the scope of the General Plan.

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The Jackson-Taylor Residential Strategy differs from the PRC in two ways. First, the Residential Strategy contains more detailed land use and urban design direction intended to implement the PRC. This direction should be followed in the development review process for zonings and planning permits. Second, the Residential Strategy identifies alternative land uses and suggests street improvements for possible inclusion in the General Plan at a later date. *Section G: Modifications to the Illustrative Land Use Plan* explains the conditions under which alternative land uses and public improvements would be considered for incorporation into the General Plan.

The Jackson-Taylor PRC indicates the locations of proposed parks, however, the ultimate size, configuration, and location will be finalized at the time of acquisition of a particular parcel. Until a park site is acquired, the land use designation of that "park" site is the PRC designation of the adjacent property in the same block to ensure development is consistent with its surroundings. The only exception is for the site proposed for the potential expansion of Bernal Park to Mission Street. If the expansion of Bernal Park does not occur at that site, then the property should develop at the High Density Residential (12-25 DU/AC) designation.

B. Housing

The Residential Strategy provides opportunities for a variety of housing within the study area. The following policies should guide the development of residential uses:

- Public and private housing funds should be used where possible to facilitate the development of affordable housing.
- Provision of ownership housing to meet the needs of a variety of income levels could be accomplished through "sweat equity," first-time home buyer programs and other mechanisms.
- Limited quantities of live/work housing are encouraged in the study area in new buildings and in existing industrial structure. The type of live/work uses allowed should be closely monitored to assure that they are safe and compatible with adjacent residential uses.

C. Parks/Community Center/Child Care

The Illustrative Land Use Plan and Illustrative Site Plan show recommended locations for parks, a day care facility and a community center. Park facilities and improvements shown are recommended, but it is recognized that actual facility selections will be made through the usual City of San José decision making process. The following policies should guide the development of these public uses:

- The City should thoroughly investigate optional funding strategies for the acquisition, construction and maintenance of the recommended parks. These funding strategies may include exactions, fees or assessment districts.
- All parks within the study area must be open to the public, however some sites may be developed in partial fulfillment of private open space requirements.
- The site for the day care facility has been located so as to allow a one acre parcel to be developed by a private, public or joint public/private day care entity.
- Although the Water and Sewer Corporation Yard at 7th and Empire streets is outside the study area, it is adjacent to the area and it is recommended that when the main corporation yard is moved, the Water and Sewer Corporation Yard also be moved and the vacant land become a neighborhood park. Since it is adjacent to the Northside Senior Center, it should be considered an alternate location for the day care center.

D. Street Improvements

The design guidelines show required development standards for all streets in the study area. The following policies should guide the development of streets:

- In many locations, new sidewalks, park strips, street trees and on-street parking configurations may be required. The City should explore optional funding sources, such as exactions, fees, or assessment districts, for both purchase and on-going maintenance of these improvements. As development occurs, the proposed street types should be implemented except where existing improvements are in good condition and are similar to the proposed street sections. In all cases however, street trees should be added.
- The implementation of angled parking will depend on City review at the time of development.
- Abandoned street segments that are reintroduced into the street grid system should either be rededicated as public streets or should be designed as private parking drives to serve surrounding development. 8th Street between Hedding and Mission and Mission Street between 7th and 8th should be rededicated as public streets.
- The plan recommends that alleys be constructed to serve the 12 - 25 du/ac Residential areas. This will require coordination between properties to obtain appropriate easements. Alleys would be privately owned and maintained. In some cases, the extensive number

of property owners in a single block prevents easy acquisition of alley easements. Therefore, alleys are highly recommended only in the following locations:

- Between Mission and Taylor from 10th to 11th streets.
- The southern portion of the block north of Mission between 10th and 11th.
- The western and northern portions of the block along 9th street between Taylor and Jackson.

In all other areas, alleyways are the preferred pattern, but may not be practical to achieve due to the number of property owners that would be required to participate in implementing the concept.

E. Conversion

The Residential Strategy is a long term plan for the study area. The implementation of the plan and the complete conversion of the area is expected to take many years. To facilitate the implementation of the plan, the following actions should occur:

- City-initiated General Plan amendments to create the Jackson-Taylor Planned Residential Community.
- City Council-initiated rezonings, consistent with the amended General Plan designations within the study area, should be processed as and when such rezonings might preserve or improve the potential for development consistent with the Strategy. Rezoning should incorporate the design guidelines set forth in the Strategy.
- The City should come to a decision on the phasing and funding for proposed parks as soon as possible.

F. Interim Uses

As the study area transitions from an industrial to primarily residential area, interim industrial uses should be regulated and monitored closely to ensure that the long term opportunities remain intact. The following policies do not apply to that area identified as "Industrial" on the land use plan, unless the land use plan changes for those properties (see Section F, "Modifications to the Illustrative Land Use Plan").

- The City will not terminate or otherwise prohibit the existing industrial businesses from maintaining their existing operations and engaging in routine property maintenance.
- All use changes within existing buildings should be consistent with the General Plan, Residential Strategy, and City codes. If a tenant leaves a building, no new uses that are less compatible with residential uses or that require a change in zoning inconsistent with the strategy will be allowed. Major changes in uses should not be allowed in any given structure. For example, a manufacturing use would not be allowed to replace a warehouse use.

- No new industrial or office buildings should be constructed in the study area, except as part of projects consistent with the General Plan and the Residential Strategy.
- Only minor building improvements should be allowed which do not change the character of the existing structure. These improvements should not add more than 25% to the pre-improvement value.
- Building renovations should not involve substantially higher quality materials or upgraded structural system types.
- The City should minimize its landscaping and off-site improvement requirements consistent with basic functional needs.
- Only minor additions to existing buildings should be allowed. These additions should not exceed 10% of the existing building square footage.

G. Modifications to the Illustrative Land Use Plan

Since the Jackson-Taylor Residential Strategy represents a long term plan for this area, the Strategy acknowledges the potential need to modify certain of its components to reflect changing conditions. These modifications should be guided by the following policies:

- All modifications should be consistent with the goals and objectives set forth in the General Plan and the Jackson-Taylor Residential Strategy.
- All land use modifications should occur through relevant planning processes such as General Plan amendments or rezonings.
- All modifications to other aspects of the strategy (e.g. street openings, landscaping, parks, etc.) should be handled within the respective established processes of the City.
- The Residential Strategy identifies several areas where alternative land uses are recommended in addition to the preferred alternative:

[S]

Area south of Jackson Street between 7th and 9th Streets (Mariani/XDM site): At the time of the City Council's adoption of the Jackson-Taylor Residential Strategy in 1992, the Mariani packing plant was an active use; however, the Strategy acknowledged that the plant may someday close and that a residential use would be appropriate. In 2001, the City Council approved amendments to the Residential Strategy and associated amendments to the General Plan to convert the use to Medium High Density Residential (12-25 DU/AC). [S]

As a result, the Mariani property is now governed by the land use policies and Design Guidelines set forth for the Medium Density Residential (12-25 DU/AC) designation. In addition, the reuse of historic brick structures and the retention of the water tower are

Deleted: Future BART station within the study area: If funding is committed for a BART station within the Residential Strategy area, these facilities (including bus stops) should be planned in a manner consistent with the Residential Strategy and compatible with the planned land uses in the study area. Efforts should be made to maximize pedestrian and bicycle access to the station. Opportunities for ground floor retail to be developed in conjunction with the transit station should be explored. If the station is located at Jackson and 7th Streets, as shown on the Illustrative Land Use Plan, and the adjacent Japantown Complex has not been completed, the City should initiate closure of 7th street to create a pedestrian plaza.

Deleted: If and when the Mariani facility is closed and not replaced by a similar industrial user, the site's General Plan designation should be amended to allow residential (12-25 du/ac) and retail on Jackson Street. Should Mariani not expand onto the XDM site within a reasonable period of time, the land use plan for that site may be changed to one or more other uses selected from the Jackson-Taylor Strategy land uses and consistent with its objectives. The City should then initiate General Plan amendments and rezonings to allow such uses on the affected sites.

strongly encouraged because of their historic significance and their contribution to the identity and character of the Jackson-Taylor area. [S]

Area south of Bernal Park between 7th and 8th Streets, north of Mission Street: This site should be used as an expansion of Bernal Park, including ultimate provisions for a day care center and community center.

Area north of Taylor Street between 10th and 11th Streets: The equivalent alternative land use for this half-block would allow neighborhood-serving commercial uses, including a potential 30,000 square foot retail establishment (e.g. "mercado" or other specialty store). A General Plan amendment would be required to establish this alternate land use (see Figure 10).

City block between Taylor and Jackson Streets and 6th and 7th Streets (City Corporation Yard/Japantown Complex site): Should this site not be developed as the Japantown Complex, it should be developed with the mixed-use development type as described in the PRC. [A/S]

Deleted: by the Nihonmachi Corporation

Area south of Taylor Street between 7th Street and the Southern Pacific rail line (parking structure site): Should this site not be used for a parking structure, it should be developed with the mixed-use development type as described in the PRC or as a park.

Southwest corner of 10th and Taylor Streets (General Commercial): Should this site not be developed with commercial businesses, it should be used for high density residential (12-25 du/ac) as defined and described in this document,

- Any modifications not addressed above or in other parts of this document will require an amendment to this document. The Planning Commission, City Council or any property owner within the Residential Strategy area may request an amendment to the Residential Strategy. The application for an amendment shall be in a form determined by the Director of Planning which includes an explanation of the proposal, the reason for the change and any necessary supporting documents, plans, etc. The proposal will be reviewed through public hearings with both the Planning Commission and City Council, however, only the City Council has the authority to amend the Jackson-Taylor Residential Strategy. [S] Applications for amendments to the Strategy from property owners will be subject to filing fees determined by the City Council.

Deleted: A proposed amendment will be considered only during the Annual Review process of the General Plan.

Deleted: designated

Deleted: Nihonmachi Survey Area

Deleted: The following policies should guide the implementation of any future Redevelopment Project Area in this area:

Deleted: ¶

- The Nihonmachi Redevelopment plan should be consistent with the General Plan:

Deleted: ¶

- Redevelopment investments are encouraged to be consistent with the Residential Strategy and should assist in meeting the goals and objectives of the Strategy, unless the Strategy is inconsistent with the General Plan.

H. Redevelopment Area

The Redevelopment Agency Board adopted a Japantown Redevelopment Plan with boundaries that are similar to the Residential Strategy study area. [S] [S] [S]

I. Environmental Review

Members of the Jackson-Taylor Task Force were particularly concerned about potential traffic impacts on surrounding neighborhood streets. Near-term environmental review, including traffic analysis and mitigations, will be completed for each project prior to rezoning and development

permit approvals. The environmental review, rezoning and development permit processes all involve public notice and public hearings.

9. Bibliography and List of Contacts Acknowledgements

The Participation of the following people provided invaluable assistance to the Jackson-Taylor Residential Strategy and is gratefully acknowledged.

Jackson-Taylor Residential Strategy Task Force

Manny Diaz, Task Force Chair
Aram Amerian, Amerian Brothers
Ken Ashizawa, Japantown Business Association
Bob Brownstein, Mayor's Office
Stan Davis, Metropolis West
Ron Harper, Hyde Park Neighborhood Association
Ron Johnson, Jackson-Taylor Neighborhood Association
Richard Kogura, Hyde Park Neighborhood Association
John Lococo, Affordable Housing Network
Art Lopez, Jackson-Taylor resident
Steve Malone, Northside Neighborhood Association
Mark Mariani, Mariani Packing Partnership
Gloria Rose Ott, San José Nihonmachi Corporation
Jerry Pendleton, Hensley Historic District
Bea Robinson, Jackson-Taylor resident
Dale Yoshihara, Japantown Business Association

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Mayor and City Council

Mayor Susan Hammer
Vice Mayor Pat Sausedo
Councilmember Blanca Alvarado
Councilmember James Beall
Councilmember Joe Head
Councilmember Nancy Ianni
Councilmember Trixie Johnson
Councilmember Shirley Lewis
Councilmember David Pandori
Councilmember George Shirakawa
Councilmember Judy Stabile

Calthorpe Associates

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Joseph Scanga, Draftsperson

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John Guisto, Recreation, Parks and Community Services
Leon Kimura, Redevelopment Agency
Calvin Matsui, Streets and Traffic
George McKissick, Neighborhood Maintenance
Joel Slavit, Recreation, Parks and Community Services
Diana Whitecar, Redevelopment Agency
Marina Yu, Housing Department

MAXIMUM INCOMES AND MONTHLY AFFORDABLE HOUSING COSTS

Number of Persons	Very Low (50% of Median)		Low (66%+/- of Median)		Median Income	Moderate (120% of Median)	
	Maximum Income	Affordable Mo. Cost @ 25%	Maximum Income	Affordable Mo. Costs @ 25%		Maximum Income	Affordable Mo. Cost @ 30% (Rentals)*
1	\$18,900	\$394	\$25,000	\$521	\$37,815	\$45,378	\$1,134
2	\$21,600	\$450	\$28,550	\$595	\$43,185	\$51,822	\$1,296
3	\$24,300	\$506	\$32,150	\$670	\$48,630	\$58,356	\$1,459
4	\$27,000	\$563	\$35,700	\$744	\$54,000	\$64,800	\$1,620
5	\$29,150	\$607	\$37,950	\$791	\$57,403	\$68,884	\$1,722
6	\$31,300	\$652	\$40,150	\$836	\$60,731	\$72,877	\$1,822

Note:

Numbers are periodically subject to change based upon modification of State and Federal law and income figures published by HUD and the California Department of Housing and Community Development

Rents to the owner will be less than the "Affordable Monthly Cost" to allow for tenant paid utilities and for a market window, because not tenant in a given income category can afford the maximum allowable.

For low and very low income owner-occupied housing, one must subtract maintenance and utility expenses, as well as property taxes, insurance, and homeowner's association fees to arrive at the amount available.

* For owner occupied housing the percentage will vary based upon applicable first lender requirements.

Effective March 1990

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